



# ***Gateways and Greenways*** **in Erie, Pennsylvania** **a Great Lake Community**

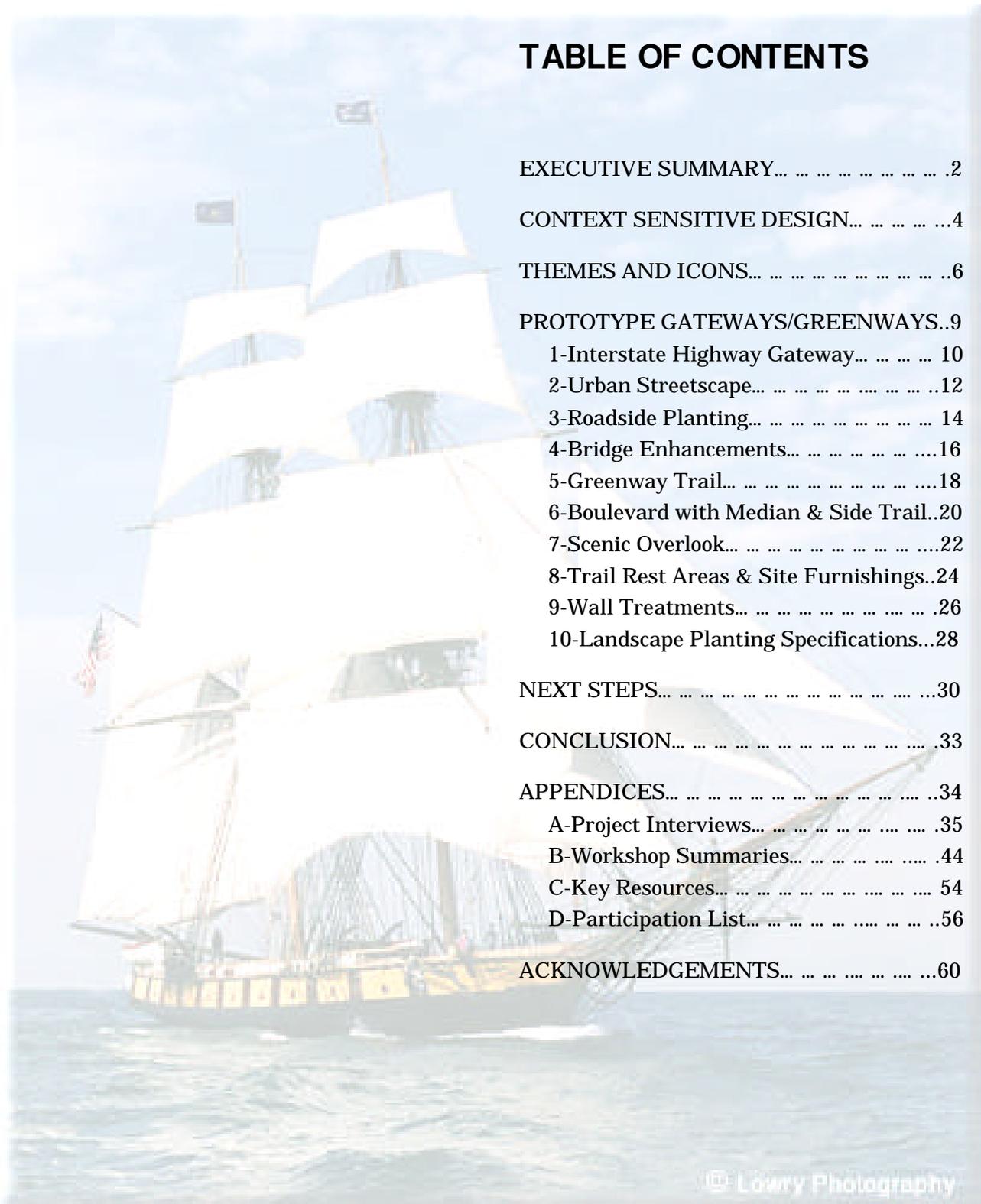


Pennsylvania Department of Transportation  
District 1-0

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E R I E



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## EXECUTIVE SUMMARY

Erie, Pennsylvania is a “**Great Lake Community.**” The region has a rich heritage, is Pennsylvania’s only port on the Great Lakes, and has community leaders who are working hard to enhance infrastructure projects through “context sensitive design.” Context sensitive design seeks to preserve and enhance the unique character and quality of a community and balance the needs of all the users of transportation projects. Erie’s character or identity is emerging and it is capitalizing on “Lake Erie” as a common theme connecting a range of important projects such as the Seaway Trail, Bayfront redevelopment, East Side Access Highway (ESAH) and other infrastructure projects. Pennsylvania Department of Transportation (PENNDOT)’s District 1-0, sponsored a workshop of Erie’s community leaders to seek their collaboration on a plan to enhance Erie’s “Gateways and Greenways.” This report is a culmination of that effort and it proposes themes, prototypes, and strategies that can be applied to future transportation projects, visitor sites and development initiatives.

The need for a regional “plan” has emerged from the implementation of extensive infrastructure redevelopment efforts that are de-

signed to enhance Erie’s ability to attract and retain visitors and improve the quality of life for residents. Erie’s community leaders recognize that with coordination these and future projects provide opportunities to incorporate unifying themes and context sensitive design. The objective of this Plan is to identify the themes, design guidelines and greenways-related concepts and to incorporate them into current and future transportation projects and visitor site improvements. The desired results are a stronger regional identity, increased pride and quality of life for residents and an increase in tourism. It is envisioned that the East Side Access Highway could serve as the prototype for incorporating themes and greenways-related concepts into transportation improvements.



Figure 1 - Possible future gateway to Erie’s Bayfront

Erie community leaders and PENNDOT designers have already given much thought to themes, icons and greenways-related concepts. To further develop these ideas and to begin to achieve consensus on priorities and process, PENNDOT engaged consultants from *Greenways, Inc.*, *Trailblazer* and *The RBA Group* (the Team) to interview and facilitate a workshop that included Erie’s community leaders and PENNDOT staff. Based on what was heard at the workshop, the Team developed this Plan that organizes workshop findings and provides a menu of options for implementing

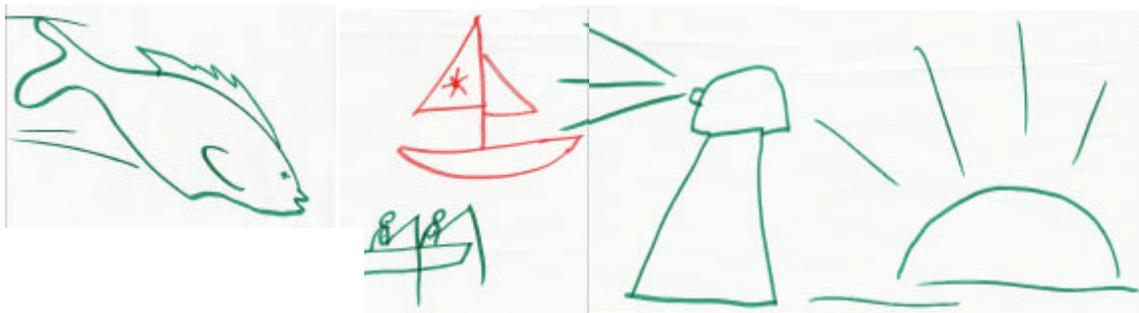


Figure 2 – Workshop sketches of icons

themes and applying greenways-related concepts to regional infrastructure projects.

Central to the success of this initiative is the concept of context sensitive design, which places emphasis on engaging the community in the process of planning transportation projects, resulting in improvements that are responsive to the community's character and needs. From the workshop, Lake Erie emerged as a central theme. Key "Lake" icons such as sailboats, fish, waves, sunsets, lighthouses, native plant species and other nautical elements relate to both the region's history and its future. These images can be integrated into infrastructure projects throughout the region. Since the nautical/Lake theme includes connecting human and natural environments, transportation projects will play a key role.



While PENNDOT has taken a lead role in facilitating this initiative, it is clear that local leadership and partnerships are critical to moving these ideas forward. The Erie Metropolitan Planning Organization, for example, might serve as a logical central point of contact or "champion" to facilitate the coordination of themes and context sensitive design into regional projects over time. A series of "early win" pilot projects have been identified which could be implemented in the short term. This report also proposes a series of prototype "gateways" and "greenways" that could serve as models for future efforts. With context sensitive design, a consistent theme, a toolbox of ideas and community partnerships, Erie, PA is prepared to enhance its unique identity through Gateways and Greenways.



**Figure 3 – A variety of streetscapes exist in Erie – some with medians, some without.**

## **CONTEXT SENSITIVE DESIGN**

There is a growing awareness throughout the nation that transportation is about more than providing for efficient and safe motorized transportation through asphalt, concrete and steel. Leading this trend is an approach to infrastructure projects called “Context Sensitive Design” which is defined by the American Association of State Highway and Transportation Officials (AASHTO) as follows:

“Context sensitive design is a collaborative, interdisciplinary approach, involving all stakeholders to ensure that transportation projects are in harmony with communities and preserve environmental, scenic, aesthetic, and historic resources while maintaining safety and mobility.” *How Transportation and Community Partnerships are Shaping America, AASHTO, 2000.*

As state agencies and communities work towards implementing context sensitive design, it also important to understand the broad implications of this approach. AASHTO describes them as:

“Rather than just designing roads to accommodate motor vehicles, [context sensitive design] aims to balance all the users of a street – pedestrians, transit riders, motorists and bicyclists. The focus is not only upon the street or road itself, but also on how these streets and roads connect to the surrounding districts and public spaces to make these areas more economically stable, safe and productive. The input of those who use and ex-



**Figure 4 – Sketch of context sensitive design applied to a boulevard in Las Vegas, Nevada**

perience a place on a regular basis is essential to the process. Moreover, to address these broader “quality of life” goals, transportation agencies and communities must work together with an open mind, and share responsibility for implementation.” *AASHTO, 2000.*

This is a significant idea coming from our nation’s transportation leadership. PennDOT’s District 1-0 has been part of this process and a willing partner to implement these concepts in partnership with the community of Erie. In the context of Erie, this approach applies not only to enhancing the motorist’s experience, but also that of the transit rider, walker and biker (*i.e.*, incorporating greenway-related concepts into the Erie’s landscape and urban fabric). Opportunities should be sought to balance the needs of all the users of a street. Just as importantly, context sensitive design is also “placemaking.” It recognizes, preserves, and enhances the unique character and quality of a community. Identifying themes and icons serves to celebrate the uniqueness of a community and enhance a visitor’s experience, as well as to instill pride among the local residents.



**Figure 5 – Public Art that portrays the uniqueness and character of a community in England**

Major projects such as the redevelopment of the Lake Erie Bayfront and the new East Side Access Highway have prompted significant interest in creating a common vision for the community. This interest has grown into this Gateways and Greenways Plan. "Gateways" can be the places where people enter and exit the Erie region and they can communicate Erie's unique character. Greenways are linear corridors of open space and they can provide recreation and transportation opportunities for nonmotorized users. They must be viewed as an integral part of the transportation network. By applying the themes, icons and greenway concepts developed through interviews and a daylong workshop with community leaders, and by focusing on a consistent approach to Gateways and Greenways, Erie's transportation infrastructure can emerge as a source of community pride.



**Figure 6 – Conceptual portrait of Erie from the Gateways and Greenways workshop**

## THEMES AND ICONS

The first step in applying context sensitive design to Erie's infrastructure is the development of a consistent theme and related icons that symbolize the character and uniqueness of the community. Throughout the interviews and workshop that preceded this Plan, the Lake served as a focal point for discussions of Erie's local history, sense of place and future development. While this might seem like an obvious conclusion, a visitor to Erie today would find an infrastructure that is largely generic, with the majority of highways and buildings designed for the suburban environment visible in almost any American town. There is little indication that Erie is a gateway to the Lake before actually arriving at Presque Isle or the Bayfront. At the same time, there are a number of exciting new projects that are moving forward to celebrate the Lake, including the Maritime Museum and the Convention Center. To realize the vision of Erie as a gateway to the Lake and to extend this imagery throughout the region, themes and icons were developed and are discussed below.

### **Themes**

"Erie, Pennsylvania: a Great Lake Community." This single phrase captures an image of Erie, PA. It can serve as a common and continuous thread throughout projects in the region, creating a context in which sensitive design can take place. An underlying nautical theme makes sense for Erie and can be readily deployed to a complete range of projects and programs including transportation projects, buildings, signage and promotional materials.

### **Icons**

When asked what icons and images symbolized their community, Erie's leaders consistently named the icons on the following list:

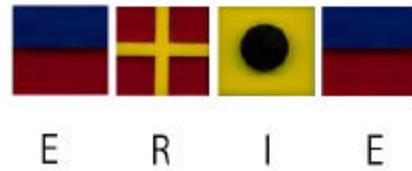
Lake Erie  
Erie Land Lighthouse  
Presque Isle  
U.S. Brig *Niagara*  
Commodore Perry Monument  
Port Buildings  
Sailboats, Ferries, Motor Boats  
Fish/Fishing  
Sunsets  
Railroads (Erie and Lackawanna)  
Seaway Trail  
Maritime Museum  
Bicentennial Tower  
Erie Zoo

These icons create an image of Erie as a "Great Lake Community." Each of them connotes a nautical theme and can connect with the region's heritage and attractions. They all relate back to the water— even the zoo can potentially include references to native plants and animals. Identifying these icons provides a basis for enhancing the concept of Erie as a lakefront community— a gateway to Lake Erie— through incorporation of these images into the local infrastructure. Including these icons into key Gateways and Greenways will be central to making Erie's infrastructure more responsive to the community's heritage, environment, economy and needs.

Take, for example, a typical roadway. In addition to the conventional concrete or asphalt motor vehicle lanes, a roadway can include striping, signage, landscaping, lighting, guide rails, transit stops, pedestrian crossings, sidewalks, bicycle facilities and traffic signals. Now imagine that the icons that make Erie unique are incorporated into the design of each of these elements. Native species would be planted as part of the highway landscape. Sculpture would be included that depicts local history and character. Images of sailboats and fish would be

cast into concrete bridge elements or applied to metal structures. Of course, any added or modified highway features should be consistent with relevant safety and design guidelines, but these guidelines can be flexible. This process illustrates a context-sensitive design approach and Erie is the context.

The next step is to make the nautical theme consistent throughout the community. Common elements such as wayfinding signage, light fixtures and transit stops offer opportunities to integrate Erie's nautical image through transportation, public works and building projects. Each application can be a mixture of 'typical' elements that are purchased in large quantities (such as signage) along with site-specific features such as a unique bridge railing. The possibilities can be as broad as the creative abilities of the community, supported by the appropriate organizational partnerships for collaboration, implementation and maintenance. The next sections of this report will provide examples of how these themes and icons can be applied in Erie, and how a coherent program of Gateways and Greenways can be implemented.



**Figure 7 – Examples of icons used in logos or proposed logos in the Erie region**



## PROTOTYPE GATEWAYS & GREENWAYS

The following fact sheets and drawings present “typical” elements of Gateways and Greenways that can be incorporated throughout the Erie region. Each represents a unique opportunity to implement the themes and icons of this initiative. It is anticipated that these prototypes are the beginning of an expanding menu of choices that can be supplemented and enhanced over time. The fact sheets describe an opportunity to create or enhance a gateway or greenway. They outline possible locations, intended character, and recommended guidelines (specifications) for implementation. The drawings graphically depict the gateways or greenways and show possible applications of the nautical theme and icons.



*Figure 8 – Images unique to Erie*

**top: aerial view of Bayfront**

**middle: vineyards northeast of Erie**

**bottom: tanker, bay and wall with bricks inscribed with the sponsors of Dobbins Landing**

## **FACT SHEET**

### **1-INTERSTATE HIGHWAY GATEWAY**

#### ***Location***

Interstate highway gateways will occur at key entry and exit points to the county, the city or special districts of Erie, such as the Bayfront, wine country or Presque Isle, in coordination with PENNDOT and the appropriate county or municipal entity. Typically gateway features could be installed at the county line, city limits, or entrance to a district.

#### ***Character***

Interstate highway gateways will contain a variety of materials including entry wall systems, entry and wayfinding signage, and landscape plants. Several options are illustrated in the drawings to show possible gateway solutions for the Erie region. Bridges that act as gateways may contain nautical flagpoles with flags and banners, and a fish or other appropriate icon appliqué on its span (see also fact sheet 4-Bridge Enhancements).

#### ***Specifications***

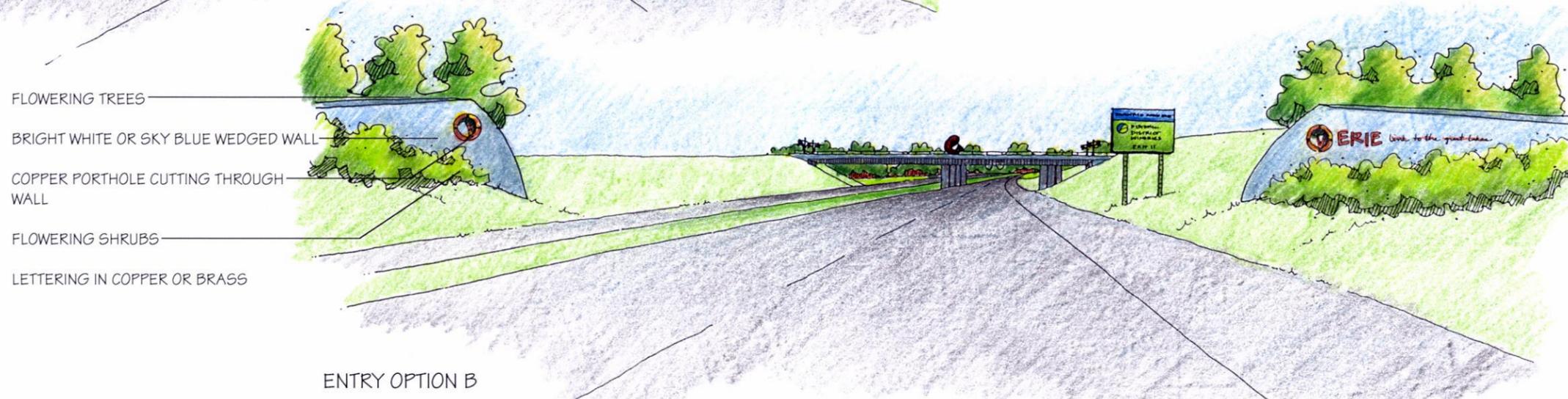
For each gateway project several elements should be present. First, entry masonry or concrete walls would be installed at strategic points along the interstate highway. The walls should be of sufficient size to stand out in the adjacent landscape and may be constructed to form a scallop or wave at the top edge of the wall. At a minimum, walls should measure 40 feet in length and stand 10 feet in height in order to be clearly visible in the landscape. Behind each entry wall, flowering or accent trees should be installed to provide an appropriate backdrop for each wall. Signage features should be installed

on the walls per the illustrative sketches. Foreground shrubs can be placed in front of each entry wall to frame the signage on the wall. PENNDOT signage should be placed at an appropriate distance from the entry walls to complement the entry feature and provide essential information. PENNDOT should avoid the installation of overhead steel structures in these landscapes to avoid obscuring the natural landscape of the Erie region. All “fixed objects” should be located outside the mandatory clear zone.



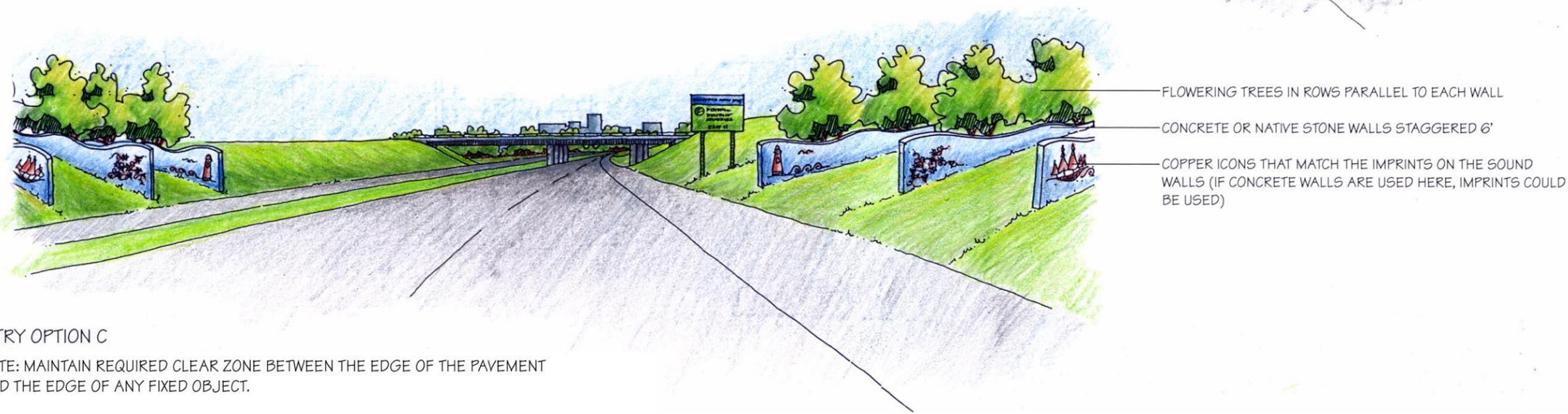
- FLOWERING TREES
- BLUE ELLIPTICAL TUBES WITH NAUTICAL LIGHT ON TOP
- GREEN METAL BACKING
- COPPER LETTERING
- PORTHOLE THAT CUTS THROUGH THE BACKING AND TUBES
- FLOWERING SHRUBS

ENTRY OPTION A



- FLOWERING TREES
- BRIGHT WHITE OR SKY BLUE WEDGED WALL
- COPPER PORTHOLE CUTTING THROUGH WALL
- FLOWERING SHRUBS
- LETTERING IN COPPER OR BRASS

ENTRY OPTION B

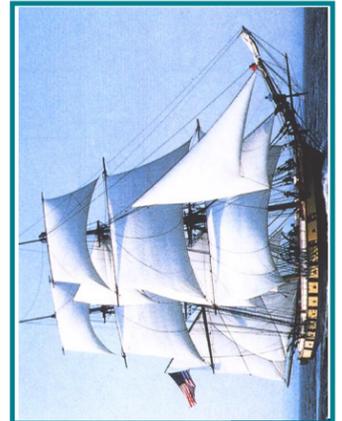


- FLOWERING TREES IN ROWS PARALLEL TO EACH WALL
- CONCRETE OR NATIVE STONE WALLS STAGGERED 6'
- COPPER ICONS THAT MATCH THE IMPRINTS ON THE SOUND WALLS (IF CONCRETE WALLS ARE USED HERE, IMPRINTS COULD BE USED)

ENTRY OPTION C

\*NOTE: MAINTAIN REQUIRED CLEAR ZONE BETWEEN THE EDGE OF THE PAVEMENT AND THE EDGE OF ANY FIXED OBJECT.

# Interstate Highway Gateway



## **FACT SHEET**

### **2-URBAN STREETSCAPE**

ticular location throughout the downtown area, these widths will vary.

#### ***Location***

Urban streetscapes are located throughout Downtown Erie and along the Erie Waterfront.

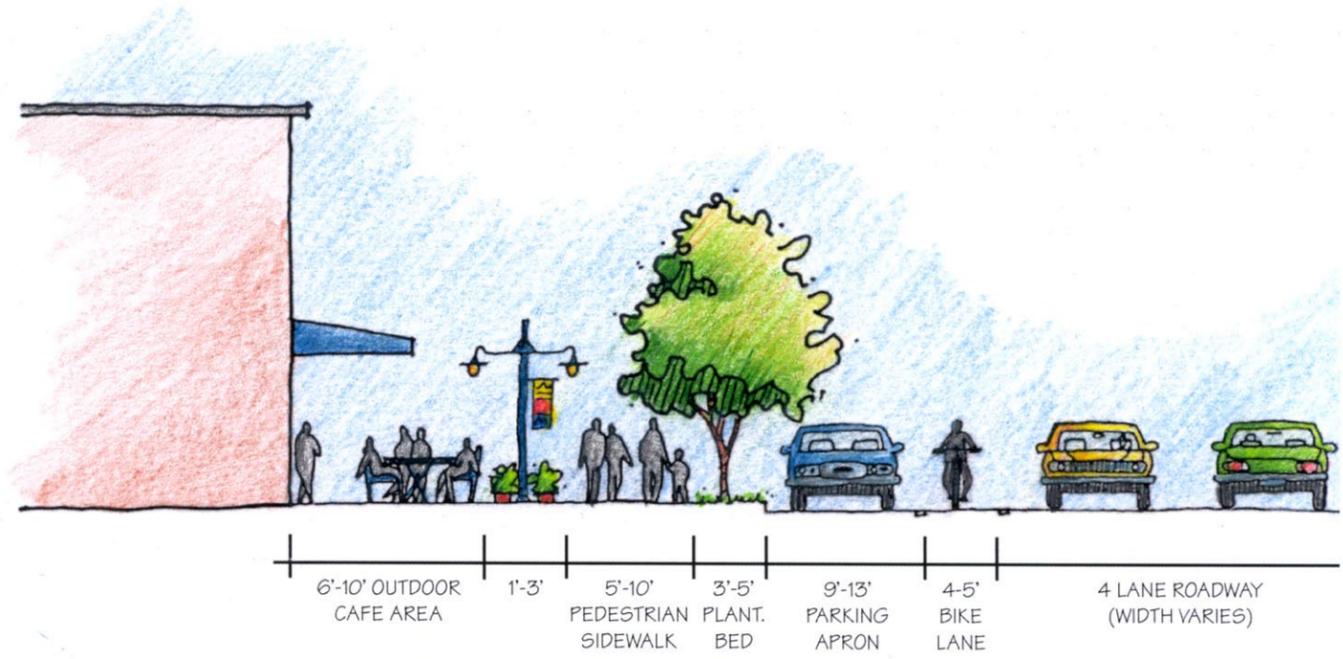
#### ***Character***

The urban streetscape theme is designed to support a more festive, outdoor atmosphere throughout Downtown Erie, which will take advantage of Erie as a destination community and improve the vitality of the downtown core. The whole street, from building edge to road centerline, can be looked at as a unique “place;” the street design not only contributes to making people feel safe, but also experiencing a sense of ownership and community. The design theme of the street should celebrate Erie’s uniqueness by expanding on the nautical themes of Erie. This means that paving materials, lighting fixtures, signage systems, banners, occasional fish sculptures, outdoor furniture and building facades should reflect the nautical heritage.

The design theme also provides function for urban streetscapes, the brick or cobble banding will support the location of public utilities into adjacent buildings and allow growth and development to occur in a manner that is not disruptive of existing streetscapes.

#### ***Specifications***

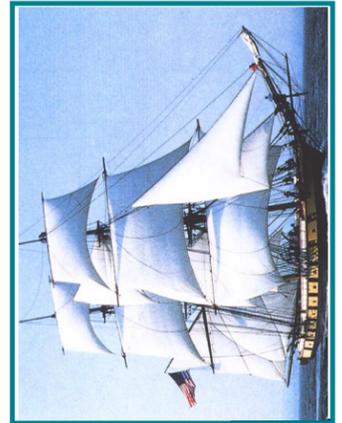
The urban streetscape consists of concrete sidewalks, brick or cobblestone banding, street trees set in brick or cobble stone root protection zones, street furniture, signage and lighting. Variable widths for walkways bike lanes, parking and public spaces are illustrated on the drawings. Given a par-



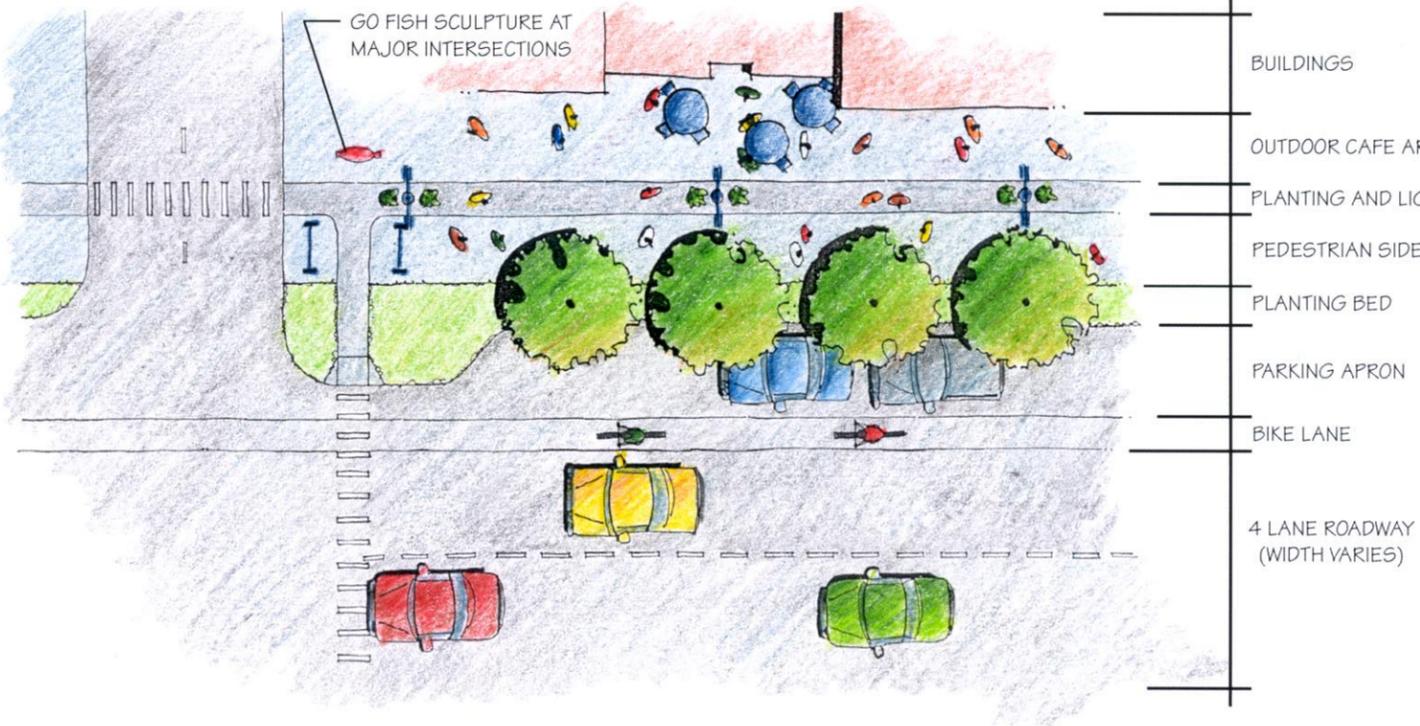
URBAN STREETScape SECTION VIEW



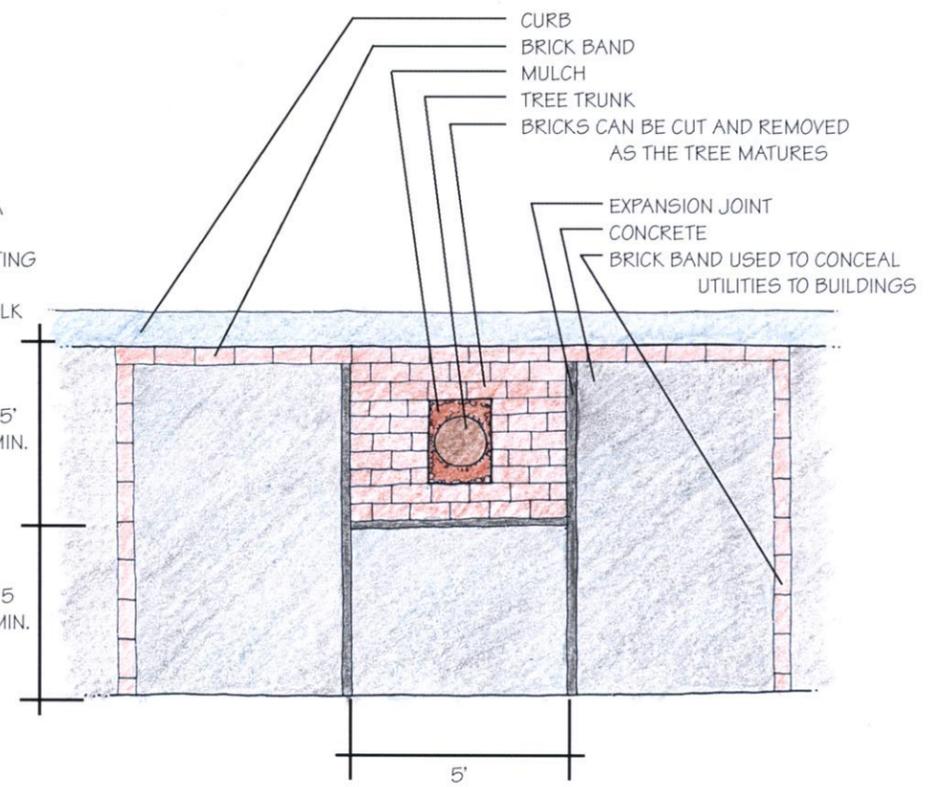
EXISTING URBAN STREETScape



\* NOTE: MORE PERMANENT GO FISH SCULPTURES CAN BE MADE OUT OF METAL AND PLACED AT IMPORTANT NODES SUCH AS TRAIL HEADS, MAJOR INTERSECTIONS, ETC.



URBAN STREETScape PLAN VIEW



SIDEWALK PLAN VIEW

# Urban Streetscape

# Erie Gateways and Greenways Design Guidelines Urban Streetscape

## **FACT SHEET**

### **3-ROADSIDE PLANTING**

#### **Location**

Roadside plantings should be accommodated wherever possible throughout the Erie region. These can occur in urban and suburban areas, as well as on rural roadways, and on roads and highways with medians.

#### **Character**

Roadside plantings should reflect the natural heritage of Erie County to the extent practical. To achieve this, plants should be selected that are relatively indigenous to the Erie region. A list of appropriate plant materials is featured below. It will be necessary and appropriate at times to use ornamental plants in settings where a more formal planting program is desired.

#### **Specifications**

For specific instructions on planting different types of plant materials, please refer to the Landscape Planting Specifications illustrative drawing.

#### **Suggested Planting Material**

##### Trees:

*Acer rubrum*, Red Maple  
*Acer saccharinum*, Sugar Maple  
*Betula allenhansis*, Yellow Birch  
*Tsuga canadensis*, Canadian Hemlock  
*Fagus grandiflora*, American Beech  
*Liriodendron tulipifera*, Tulip Tree  
*Platanus occidentalis*, Sycamore  
*Quercus alba*, White Oak  
*Quercus palustris*, Willow Oak  
*Quercus rubra*, Northern Red Oak

##### Shrubs:

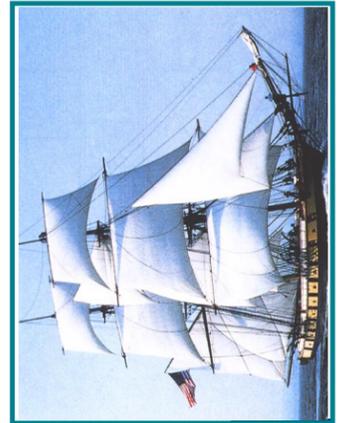
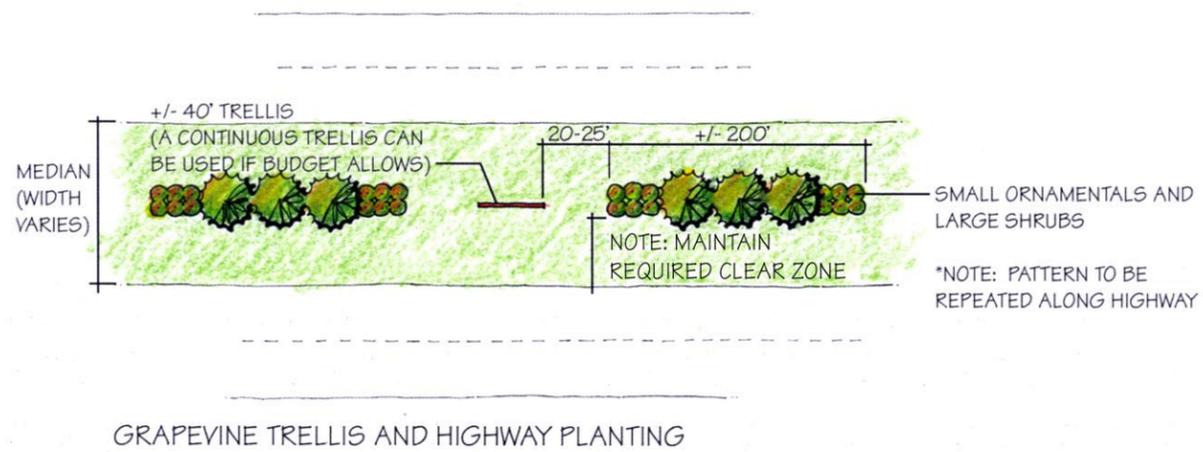
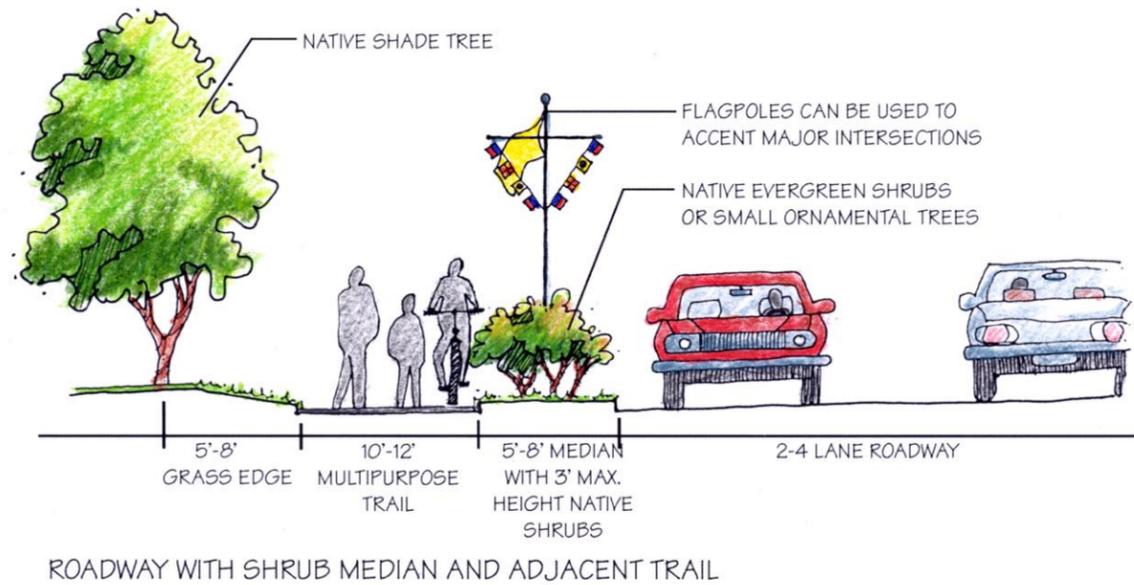
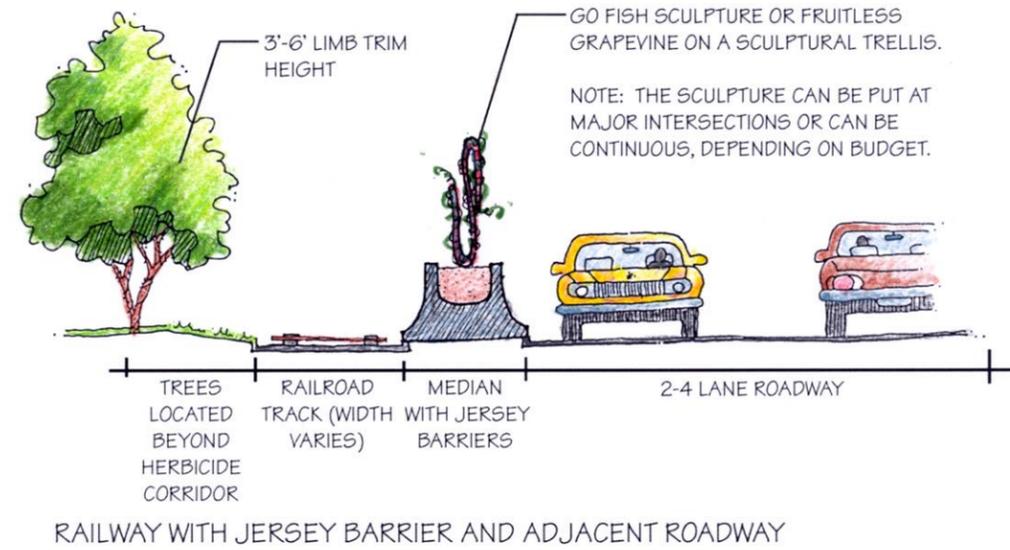
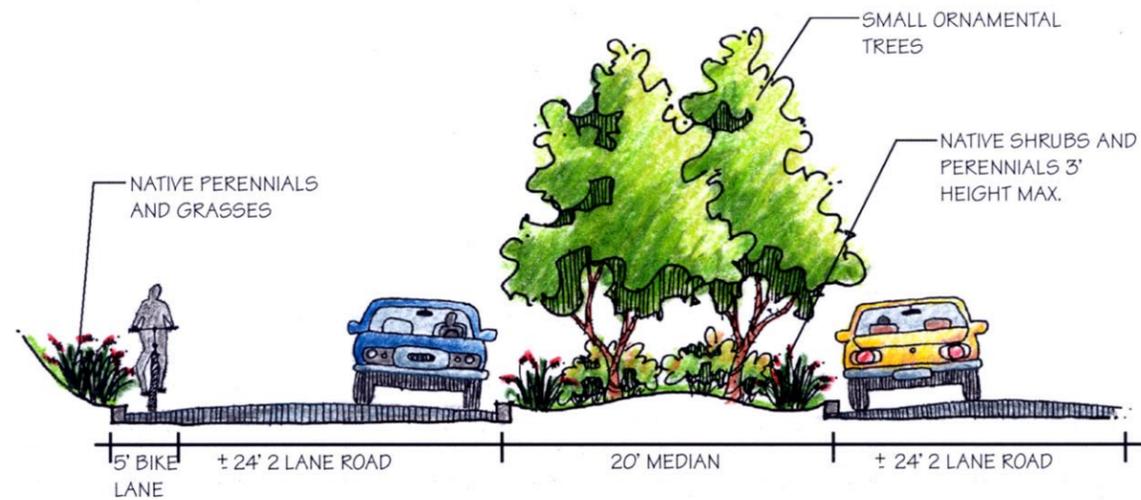
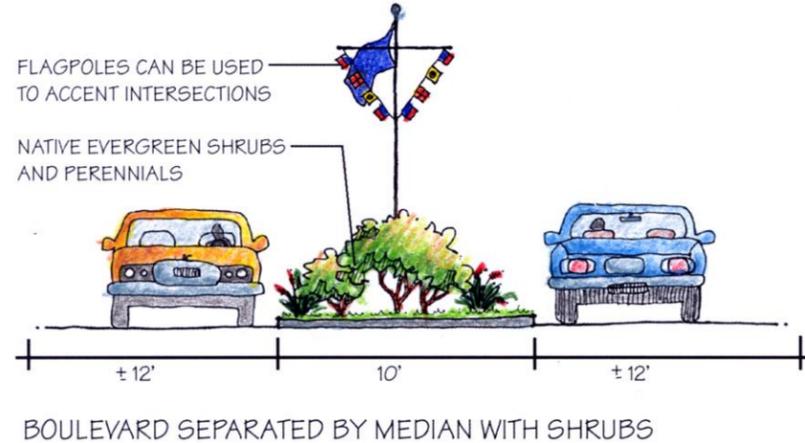
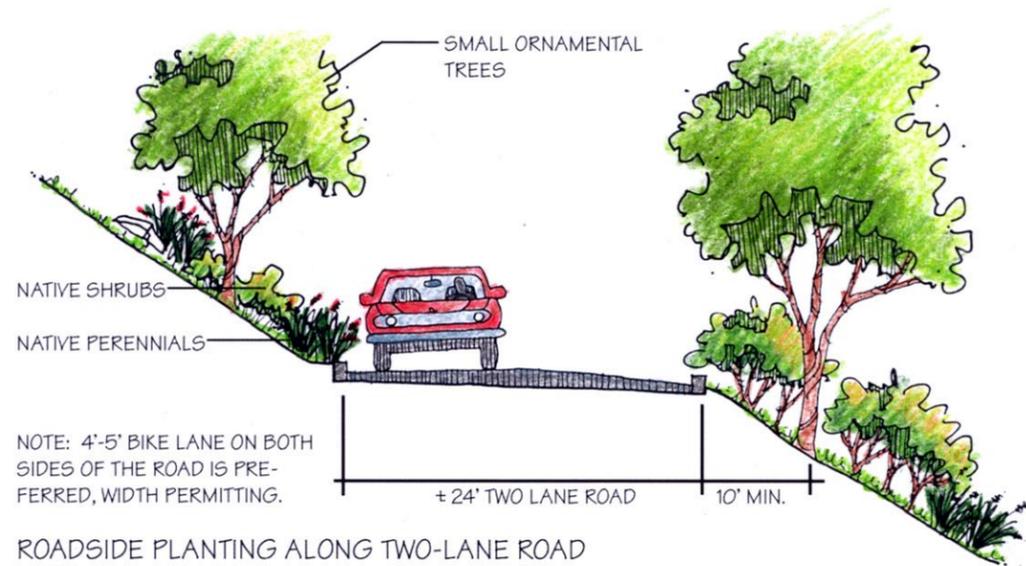
*Aronia melanocarpa*, Black Chokeberry  
*Ceanothus americanus*, New Jersey Tea  
*Hydrangea arborescens*, Hydrangea  
*Rhododendron periclymenoides*, Wild Honeysuckle  
*Viburnum acerifolium*, Mapleleaf Viburnum

##### Perennials:

*Aquilegia Canadensis*, Wild Columbine  
*Arisaema triphyllum*, Jack-in-the-Pulpit  
*Asarum Canadensis*, Canada Wild Ginger  
*Asclepias syriaca*, Common Milkweed  
*Asclepias tuberosa*, Butterfly Weed  
*Aster novae-angliae*, New England Aster  
*Helianthus spp.*, Sunflower  
*Heliopsis helianthoides*, Oxeye Sunflower  
*Phlox divaricata*, Phlox  
*Rudbeckia spp.*, Coneflower  
*Vitis*, Grape Vines

##### Grasses:

*Elymus tristrix*, Bottlebrush Grass  
*Panicum virgatum*, Switchgrass  
*Schizachyrium scoparium*, Little Blue-stem  
*Sorghastrum nutans*, Indian Grass



# Roadside Planting

## **FACT SHEET**

### **4-BRIDGE ENHANCEMENTS**

#### ***Location***

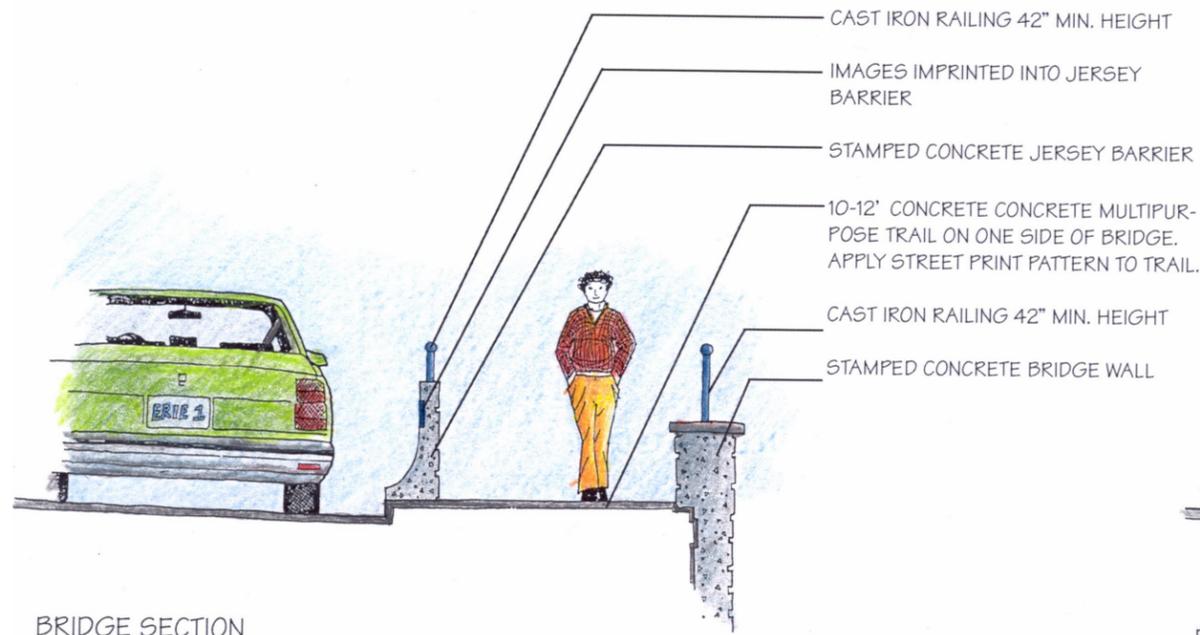
Bridge enhancements are specifically focused on the Wintergreen Gorge Bridge as a prototype; however, the design solutions are applicable for other bridge projects throughout the Erie region, including bridges spanning roads and highways.

#### ***Character***

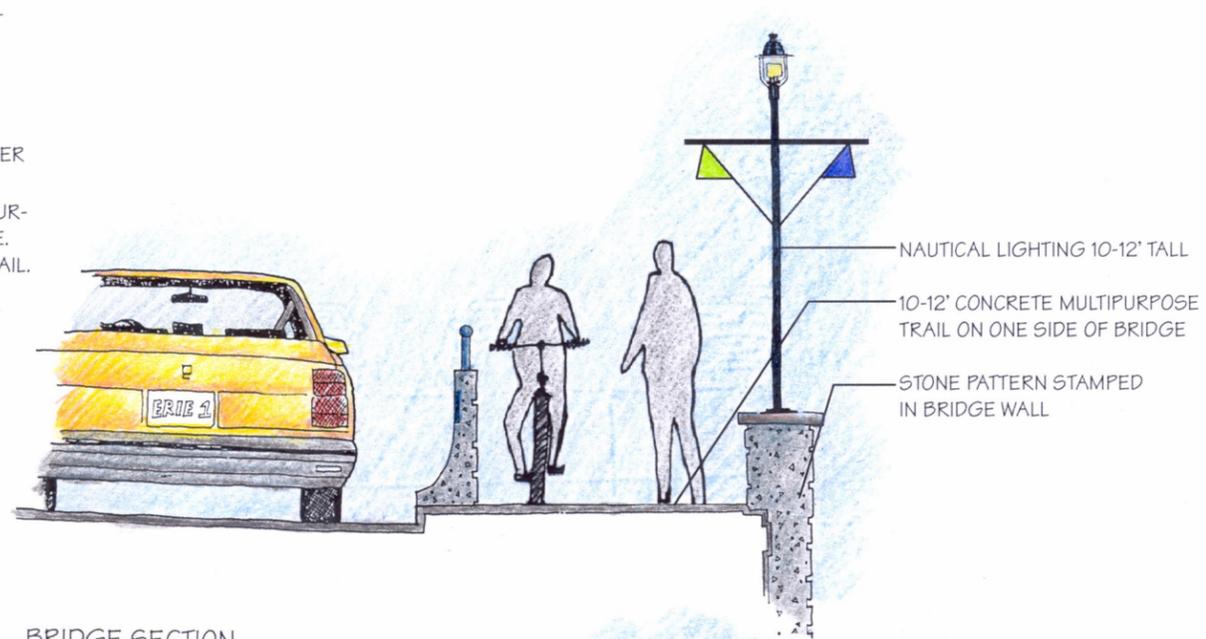
The principal function of these recommendations is to add aesthetic appeal to the plate girder design that will be implemented over the Wintergreen Gorge bridge project.

#### ***Specifications***

The bridge enhancements consist of modifying the Jersey barrier design so that it contains features that are unique to the Erie region. These features include adding a stamped concrete band to the inside (roadway side) of the Jersey barrier that incorporates the themes that have been used on the wall treatments. The exterior (Gorge side) of the Jersey barrier would be stamped in a stone or brick pattern so that it is visible from the floor of the Gorge. Light columns or posts in the nautical theme will need to be installed at regular intervals along the bridge. PENNDOT engineers should determine the interval. A fish or other appropriate appliqué can be mounted on the span of a bridge (for instance, a fish could be leaping over the gorge or a highway could be passing under a sunset).



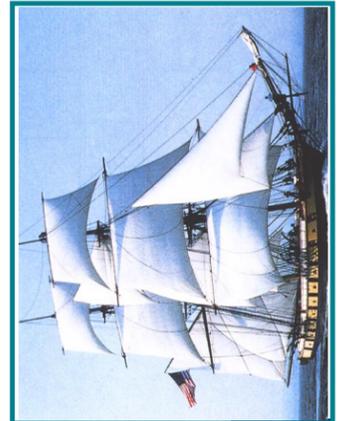
BRIDGE SECTION



BRIDGE SECTION



WINTERGREEN GORGE BRIDGE



## **FACT SHEET**

### **5-GREENWAY TRAIL**

#### **Location**

Greenway Trails can be located throughout the community, and are currently identified within the *Erie County Trails and Greenways Plan*. Greenway trails are typically found within independent off-road rights-of-way, along riparian corridors, abandoned rail corridors, utility corridors or specially constituted public rights-of-way. Greenway trails can extend parallel to roadways as sidepaths, if there are few roadway crossings where potential turning vehicle conflicts might occur. If large numbers of turning vehicles were present on a roadway, it would be better to install an on-road bike facility and sidewalk combination. Trail gateways would be located at key entry and exit points, such as when the Seaway Trail crosses the county line or a regional trail enters the city limits.

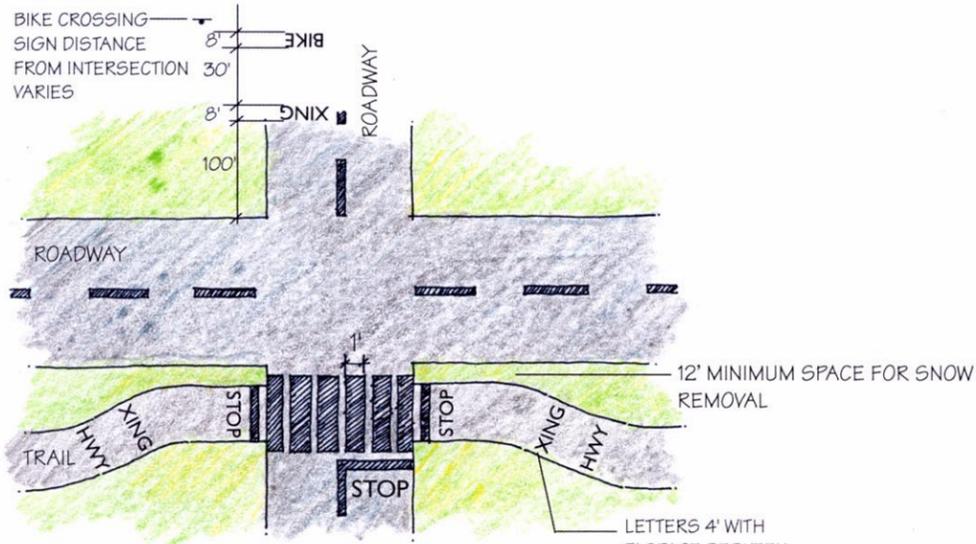
#### **Character**

Most greenway trails would be characterized as free flowing in both form and function. Various unique Erie themes can be expressed within greenway trails by the manner in which trails are designed and linked to popular destinations. For example, through design, various imprints in the pavement surface can be used to express nautical themes of the area. A green line can be painted on the land side of the trail and a blue line can be painted on the water side to coordinate with the proposed way-finding signs. Trails can have interpretive signage that provides information about the history and culture of the community. Furnishings, furniture and trail amenities can be added that define a sense of place that is uniquely Erie. Trails can also have gateway components, similar to the "Interstate

Highway Gateway" (see Fact Sheet 1-Interstate Highway Gateway).

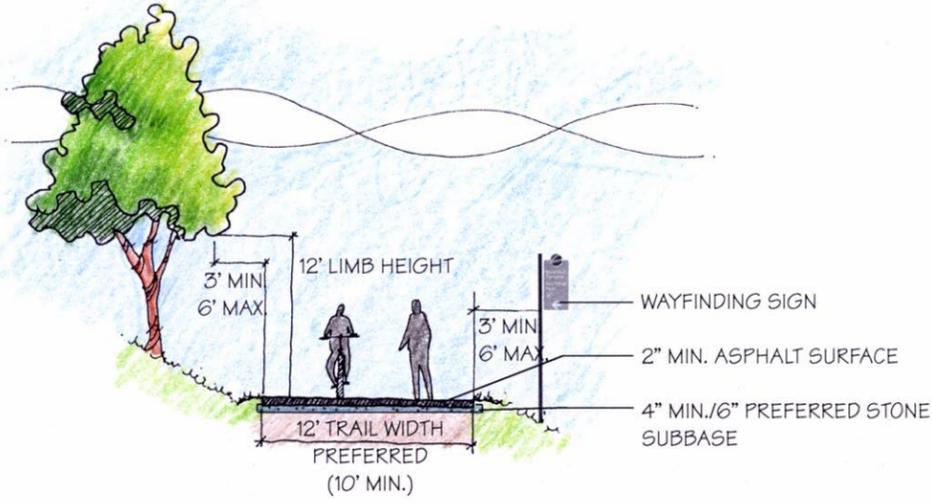
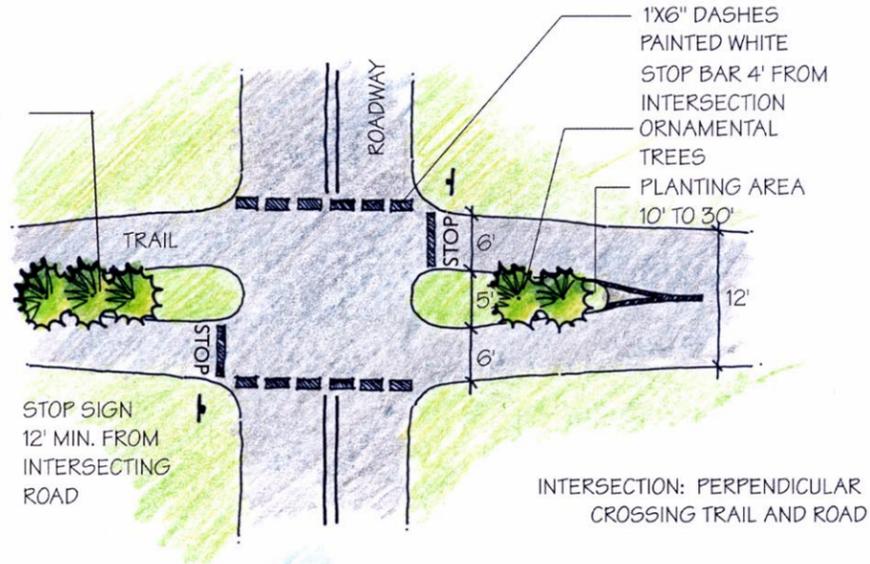
#### **Specifications**

Greenway trails can be surfaced with concrete, asphalt or crushed stone. Trail design should be influenced by projected use. For example, if a trail is designed to support multiple users, including persons with disabilities, a hard surface trail is preferable. If a trail were designed to extend through an environmentally sensitive landscape, a soft or native surface would be appropriate. Trails that support two-way bicycle traffic should be no less than 10 feet wide, with a preferred width of 12 feet. Pedestrian-only foot trails can be 6 to 8 feet wide. When locating a sidepath along a roadway, a minimum of 12' should be maintained between curb and path, for maintenance, safety and aesthetic purposes.

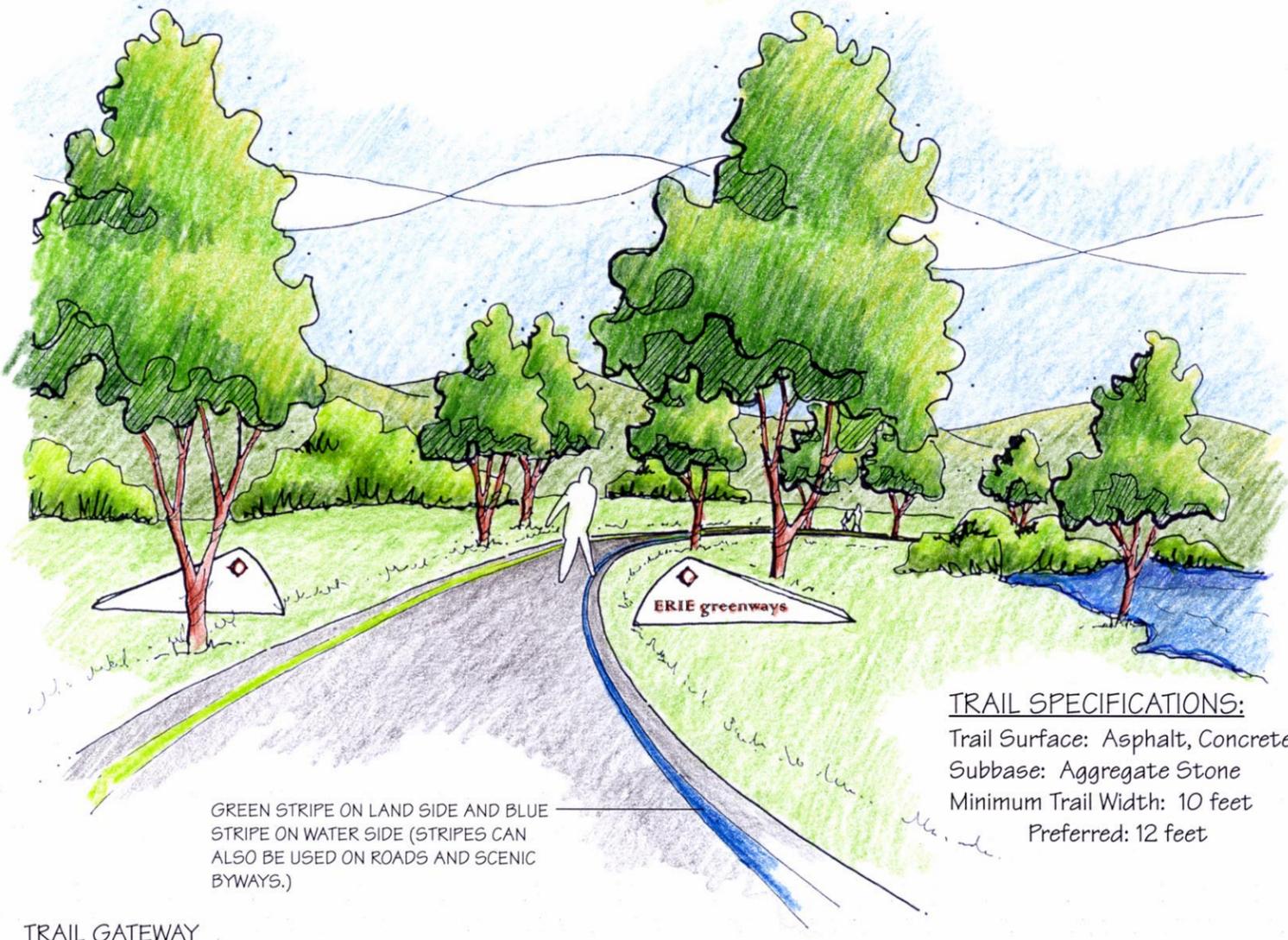


INTERSECTION: PATH ADJACENT TO ROADWAY

COULD PLACE A GO FISH SCULPTURE AT MAJOR INTERSECTIONS



TRAIL CROSS SECTION



TRAIL GATEWAY

**TRAIL SPECIFICATIONS:**  
 Trail Surface: Asphalt, Concrete  
 Subbase: Aggregate Stone  
 Minimum Trail Width: 10 feet  
 Preferred: 12 feet



## **FACT SHEET**

### **6-BOULEVARD WITH MEDIAN & SIDE TRAIL**

#### ***Location***

Boulevards with medians and side trails can be installed in a number of locations throughout the Erie region, specifically along the waterfront and on other principal entry roads into the community.

#### ***Character***

Boulevards with medians and side trails should offer a “parkway” character, reminiscent of the early 20th century construction, which features tree-lined roadways. Ample space should be provided for all project elements, including room for large shade trees to grow and a successful side path trail system to be developed.

#### ***Specifications***

Boulevards should be designed for motor vehicle movement in accordance with PENNDOT specifications. Where possible, large shade trees should be installed in planting medians that are a minimum of 10 feet wide from back of curb or edge of pavement. Smaller trees or shrubs may be used to meet safety recommendations. The side path trail system should function as a two-way bicycle and pedestrian travel way, with a minimum width of 10 feet. The side path should be paved with concrete or asphalt, per PENNDOT specifications.

ORNAMENTAL TREES (PATTERN TO BE REPEATED)  
SEE TREE SPECIES LIST ON ROADSIDE AND TRAIL  
PLANTING PAGE FOR A LIST OF POSSIBLE TREES

SHRUBS CAN ALSO BE ADDED TO THE MEDIAN,  
DEPENDING ON THE WIDTH AVAILABLE

ERIE FLAGPOLES

BIKE LANE

BIKE LANE

NAUTICAL LIGHTING (CAN BE THE SIMILAR TO EXISTING  
BAYFRONT LIGHTING.) NAUTICAL FLAGS CAN BE ADDED IF  
LIGHTS ARE LOCATED NEAR WATER.

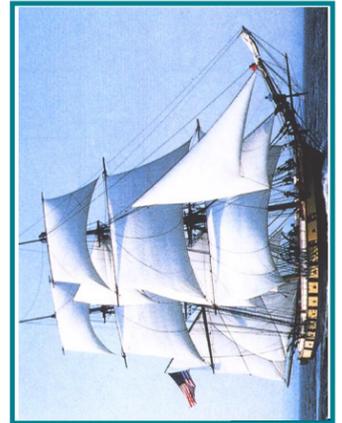
ORNAMENTAL TREES AT MAJOR INTERSECTIONS

FLOWERING EVERGREEN SHRUBS AND PERENNIALS AT  
MAJOR INTERSECTIONS

THEMES (STREET PRINT) CAN BE STAMPED IN CONCRETE TO  
WARN OF APPROACH TO INTERSECTIONS

WAYFINDING SIGNAGE

8' SIDEWALK



# Erie Gateways and Greenways Design Guidelines Boulevard with Median and Side Trail

Boulevard with Median and Side Trail

## **FACT SHEET**

### **7-SCENIC OVERLOOK**

#### ***Location***

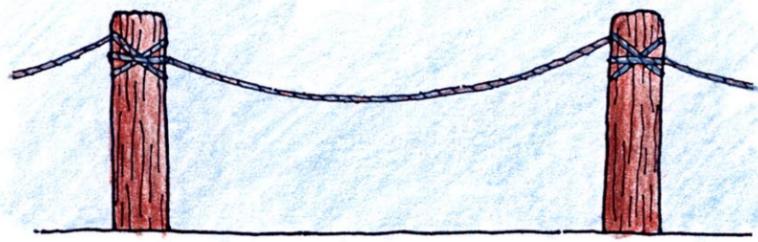
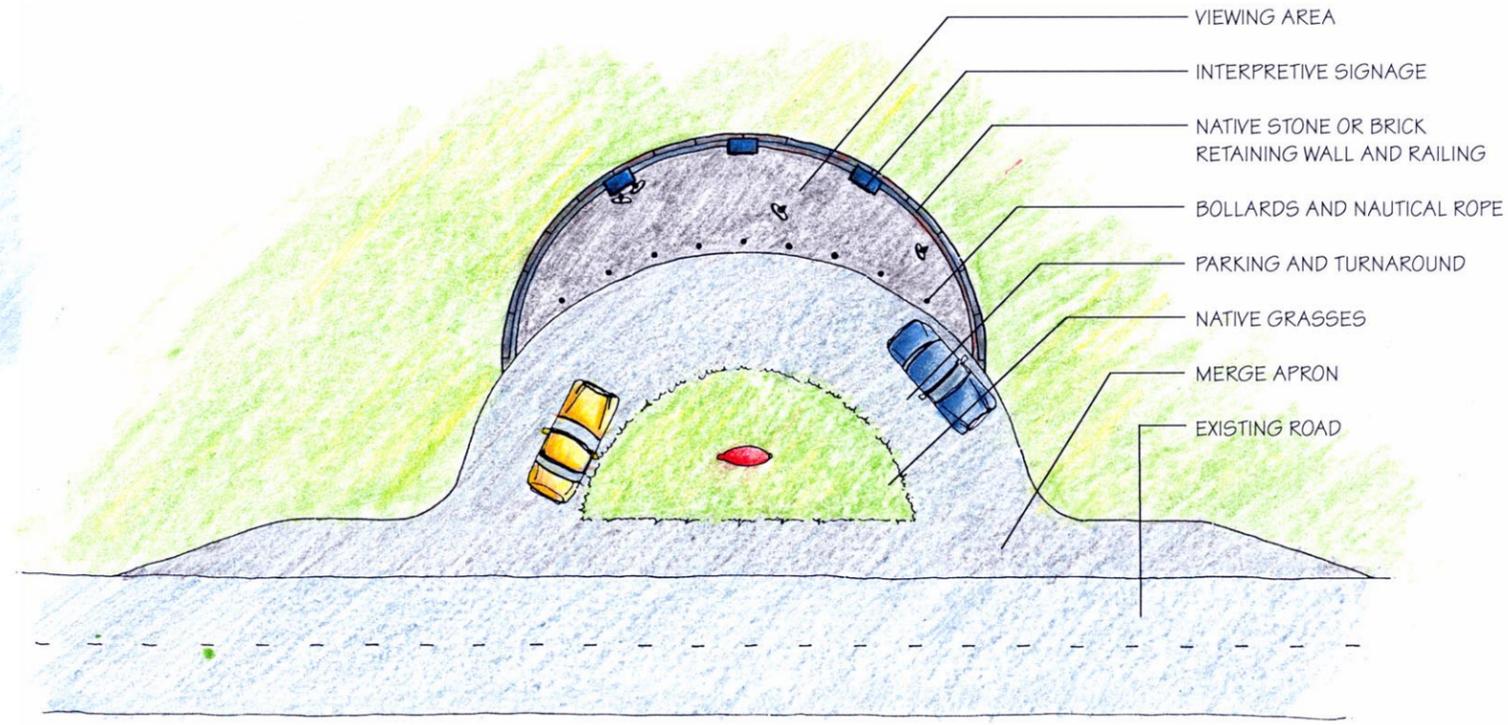
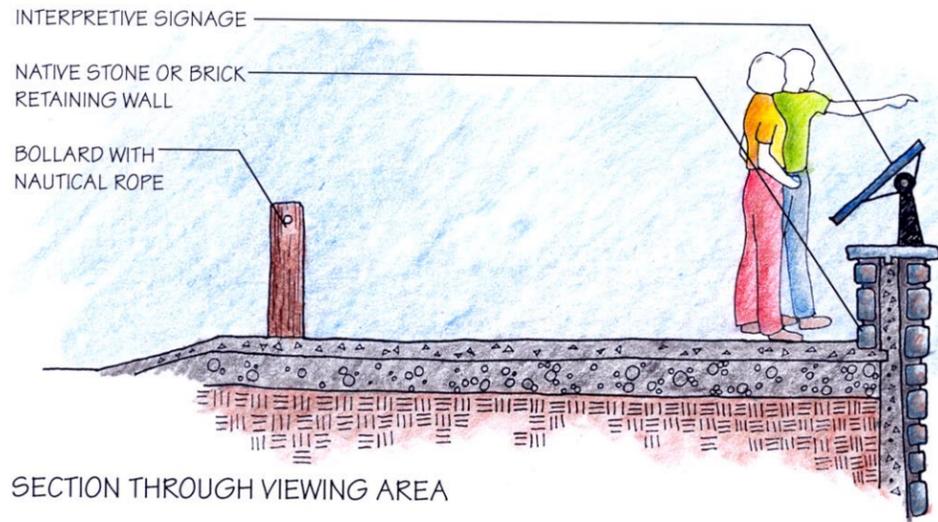
Principally, scenic overlooks should be located in landscapes adjacent to roadways where a view is possible to Lake Erie or other significant natural features.

#### ***Character***

The concept of the scenic overlook is to encourage motorists to stop, get out of their cars, orient to the surroundings and take in the view of the adjacent landscape. Interpretive signage should be installed at the overlook to educate visitors about the view or surrounding landscape heritage. Native materials should be used to build the support facilities (stone walls, walking surfaces, bollards, etc) for the overlook.

#### ***Specifications***

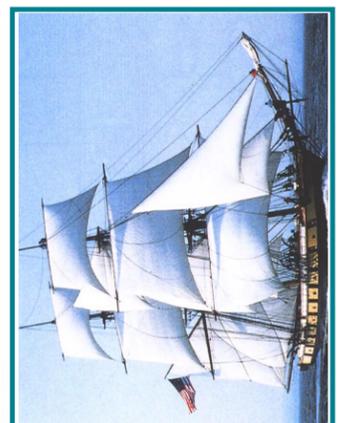
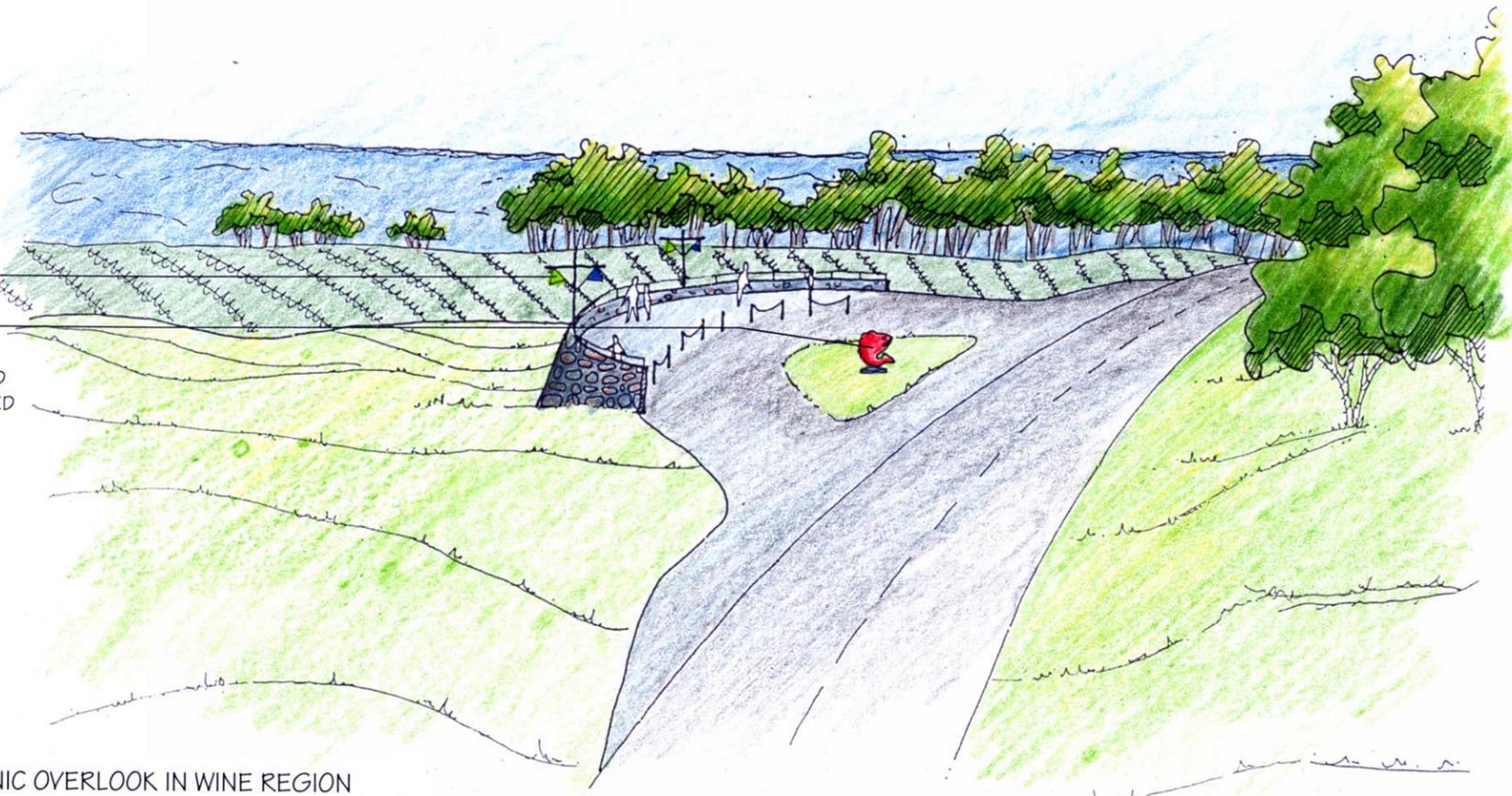
Scenic overlooks should be built to fit into the surrounding landscape and be as unobtrusive as possible. The overlooks should be constructed with proper ingress and egress lanes from the adjacent roadway environment, in accordance with PENNDOT specifications. Overlooks should support parking for a maximum of five automobiles, avoiding the appearance of a large parking lot. Viewing platforms should be supported, as necessary, by retaining walls so that they provide for a level transition area from the auto parking area. Bollards should be installed to separate parked cars from the viewing platform. Interpretive signage should be installed to describe the views or natural landscapes of the surrounding area. Nautical flagpoles can be used if the overlook is near the water. Fish sculptures can signal the visitor's arrival to the Erie region.



NAUTICAL FLAGPOLE

GO FISH SCULPTURE

\*NOTE: THE PARKING AREA AND TURNAROUND CAN BE ADJUSTED ACCORDING TO THE SITE



## **FACT SHEET**

### **8-TRAIL REST AREAS & SITE FURNISHINGS**

#### ***Location***

Trail rest area site furnishings should be installed at trail rest areas. Trail rest areas should be positioned at about 1 to 2 mile intervals along a typical trail project. Also shown are other furnishings with nautical themes that can be used in various applications throughout the region.

#### ***Character***

Trail rest areas should reflect the heritage of Erie. This can be accomplished by installing furniture and furnishings that reflect the nautical themes of Erie. Furniture and furnishings should include trail lighting, benches, trash receptacles, bike racks, drinking fountains (to represent the nautical theme, could be in the form of water butts) and signage.

#### ***Specifications***

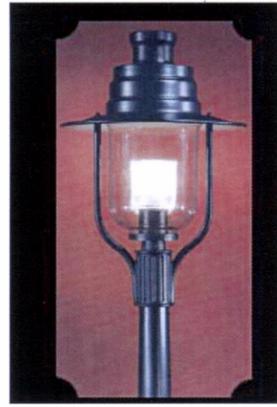
Each rest area should be designed to comfortably seat a minimum of four people simultaneously. The rest areas should be positioned adjacent to the trail edge, but outside the travel way of the trail. The specifications for individual elements of a rest area are featured in the illustrative sketch. If concrete is used to pave the rest areas, "streetprints" (impression stamped in the concrete while wet) with a nautical theme can be incorporated into the concrete.



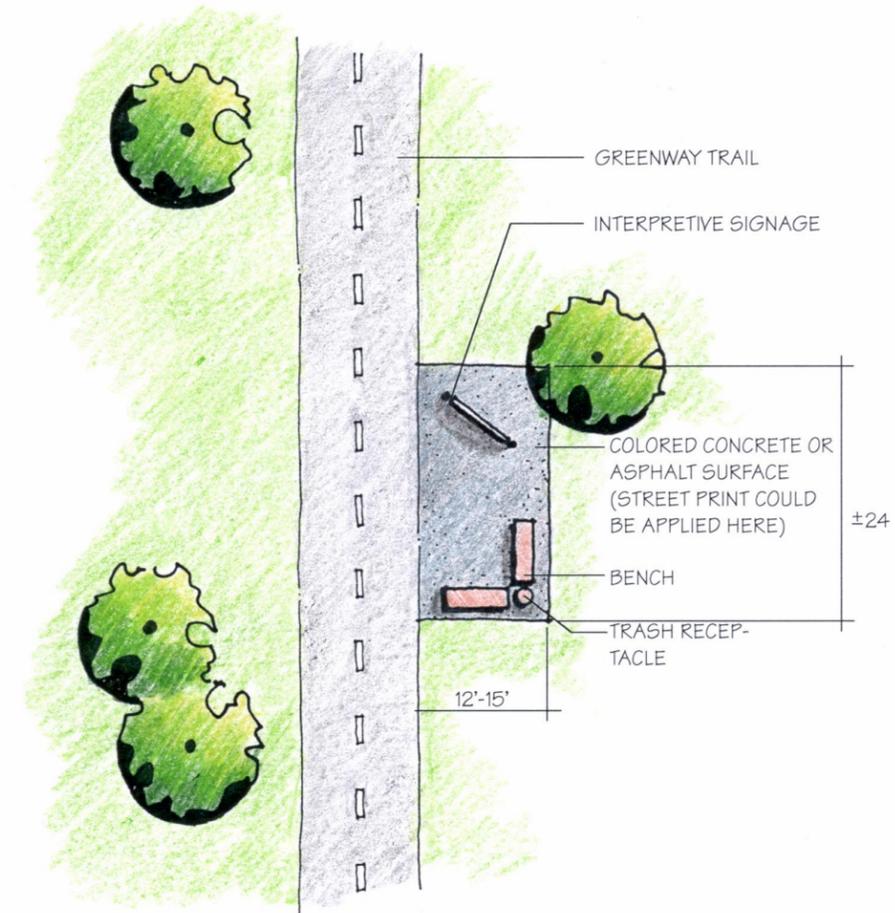
WAYFINDING SIGNAGE



LIGHTING WITH NAUTICAL FLAGS



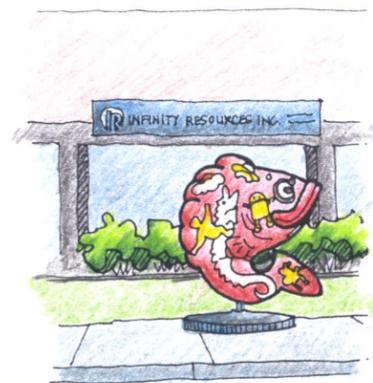
LIGHTING



TRAIL REST AREA PLAN VIEW



TRASH RECEPTACLE



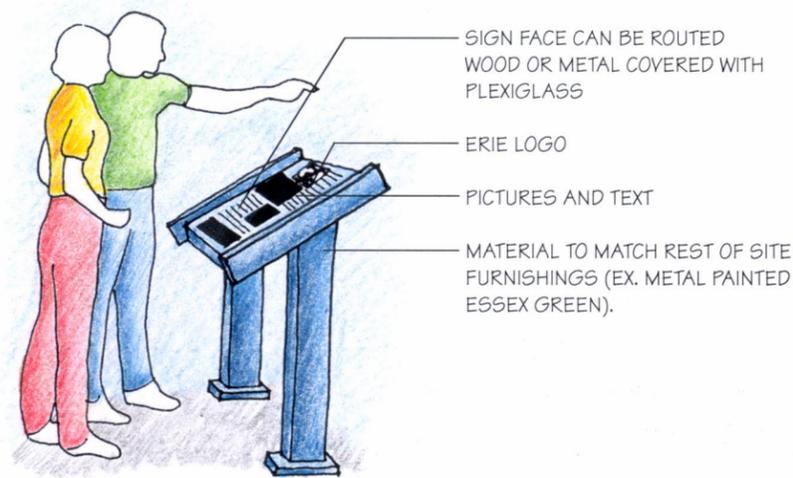
GO FISH SCULPTURE



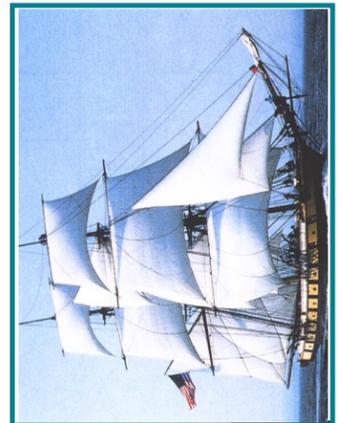
BIKE RACK



WATER FOUNTAINS



INTERPRETATIVE SIGNAGE



Erie Gateways and Greenways  
Design Guidelines  
Trail Rest Areas and Site Furnishings

Trail Rest Areas and Site Furnishings

## **FACT SHEET**

### **9-WALL TREATMENTS**

#### ***Location***

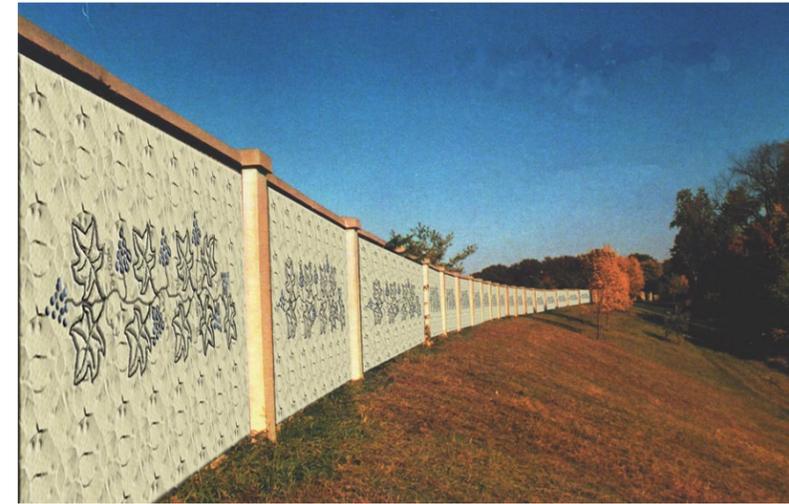
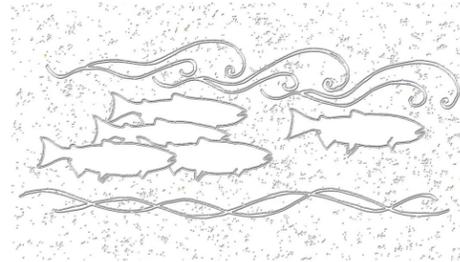
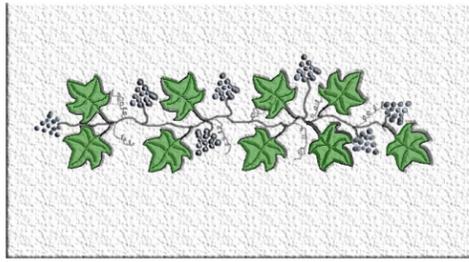
Highway sound barrier walls will be located on specific projects as defined by the project engineer. Retaining walls will be commonly used throughout the region to retain earth and transition grade and they can incorporate the nautical features shown on the drawings.

#### ***Character***

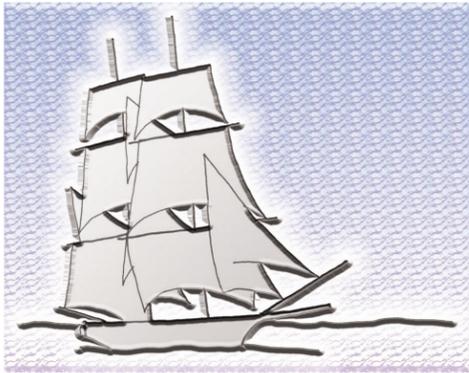
PENNDOT and FHWA standards will determine the height of the highway sound barrier walls. They can be constructed out of poured-in-place concrete, metal, brick, glass or concrete block. They can also be prefabricated from the same materials and delivered to site and installed per the project engineers specifications.

#### ***Specifications***

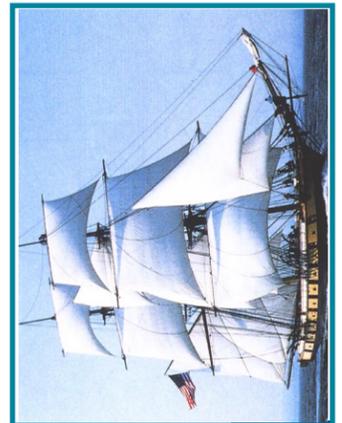
The patterns on the retaining and sound barrier walls are intended to reflect the nautical themes and heritage of the Erie region. These patterns can be achieved through various methods. For example, they can be created using plastic patterns stamped into wet concrete. Color can be mixed into the concrete before the pattern is applied or as a stain to the concrete after it has been stamped and dried. The patterns illustrated in the drawings can be applied to every 5th or 10th pattern, as specified by project engineer.



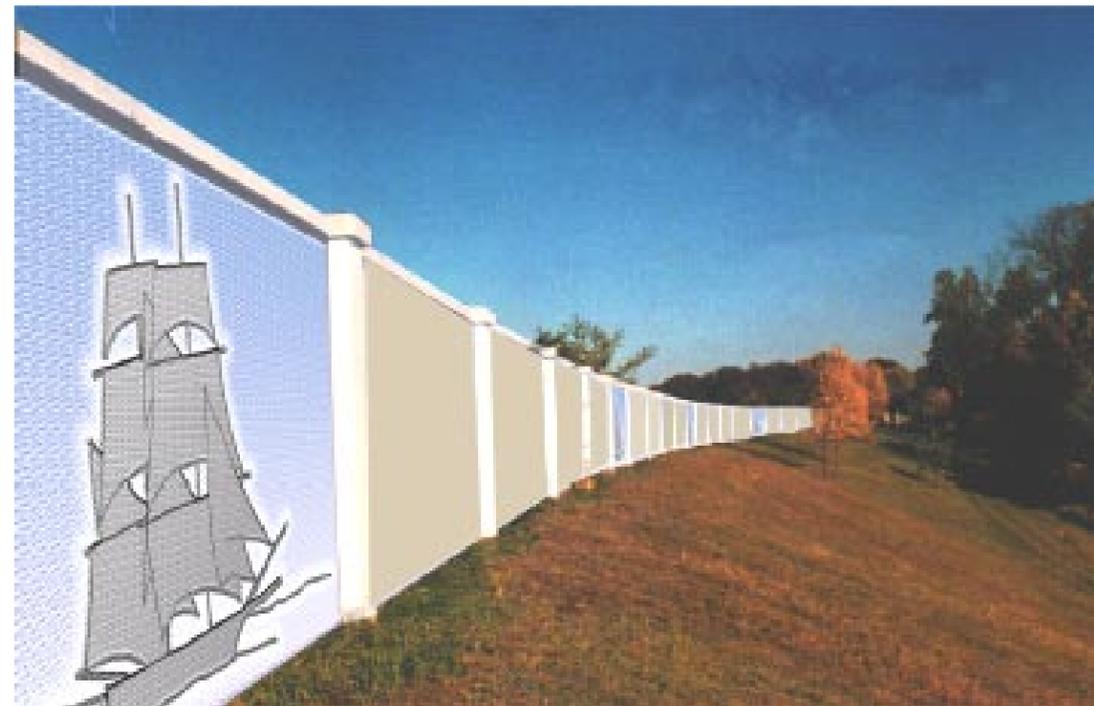
GRAPE VINE SCHEME



PATTERN ALTERNATIVES



STEELHEAD SALMON SCHEME



SAILBOAT SCHEME

\*NOTE: IMAGES CAN BE USED ON EVERY PANEL OR THEY CAN BE ALTERNATED WITH PLAIN PANELS.  
IMAGES CAN BE USED ON RETAINING WALLS, PLANTERS, JERSEY BARRIERS, ETC.  
IMAGES WILL BE STAMPED INTO WET COLORED CONCRETE OR STAINED CONCRETE, NOT PAINTED.

# Wall Treatments

# Erie Gateways and Greenways Design Guidelines Wall Treatments

## **FACT SHEET**

### **10-LANDSCAPE PLANTING SPECIFICATIONS**

#### ***Location***

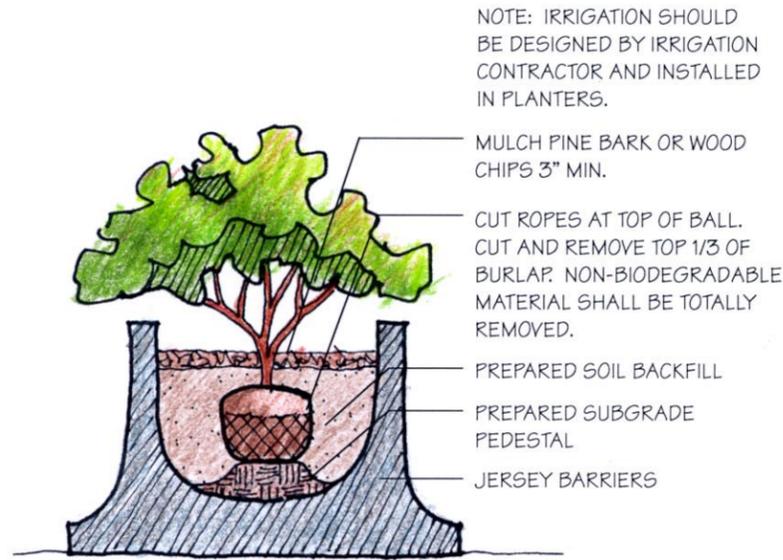
Landscape plantings can be incorporated into all project areas within the Erie region. Plantings are intended to complement, accentuate and improve the aesthetic appearance and ecological function of the landscape. Therefore, the specific location of plantings should reflect the landscape heritage of Erie. To the extent possible, landscape plantings should be installed in a manner that supports the establishment of a healthy and stable ecosystem.

#### ***Character***

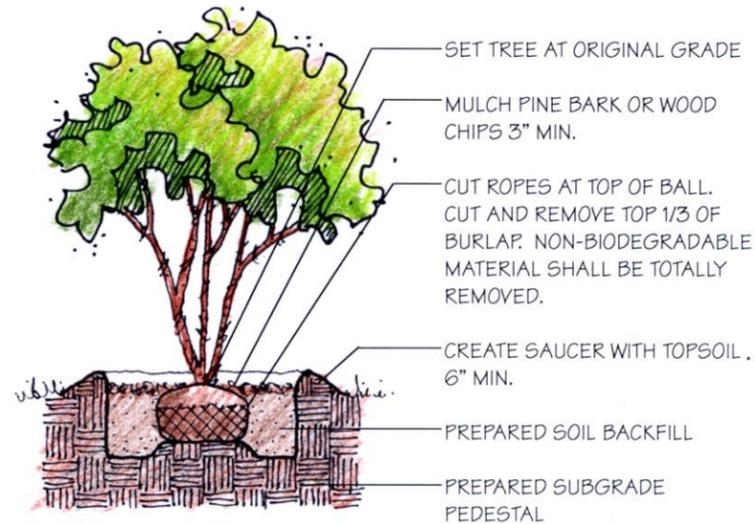
Landscape plantings should be situated in the landscape in locations and arrangements that reflect the natural habitat of indigenous plants. Naturalizing the landscape form should be the goal for landscape plantings. At times, a formal planting plan may be necessary due to the width of the planting bed and the desired arrangement of plants in relationship to other landscape features.

#### ***Specifications***

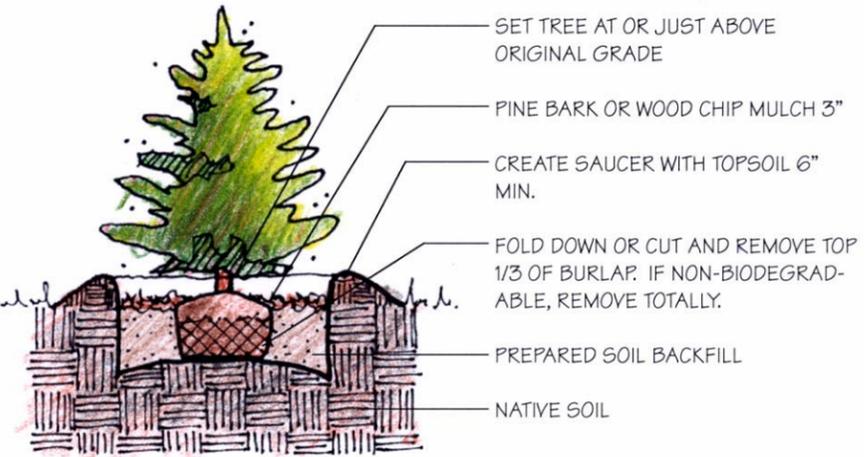
A variety of native plants should be used whenever possible for landscape plantings. However, certain projects will call for the installation of ornamental plants to draw particular attention to a landscape feature or to signify a transitional zone in the landscape. Landscape planting details are shown on the drawings. Maintenance should be a chief concern in the selection and installation of the landscape plantings.



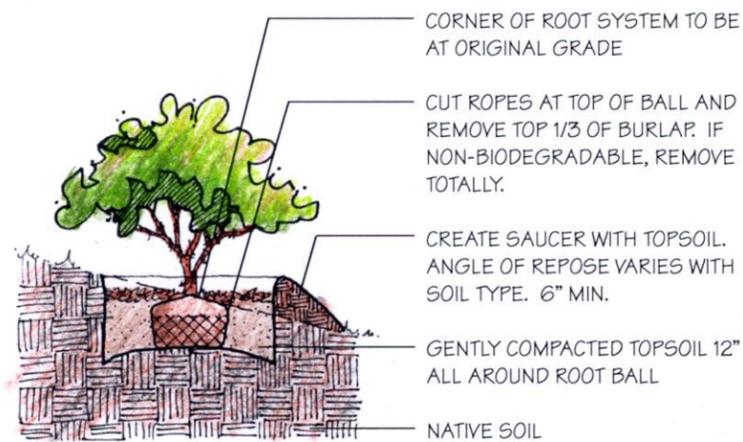
SHRUB WITHIN JERSEY BARRIER



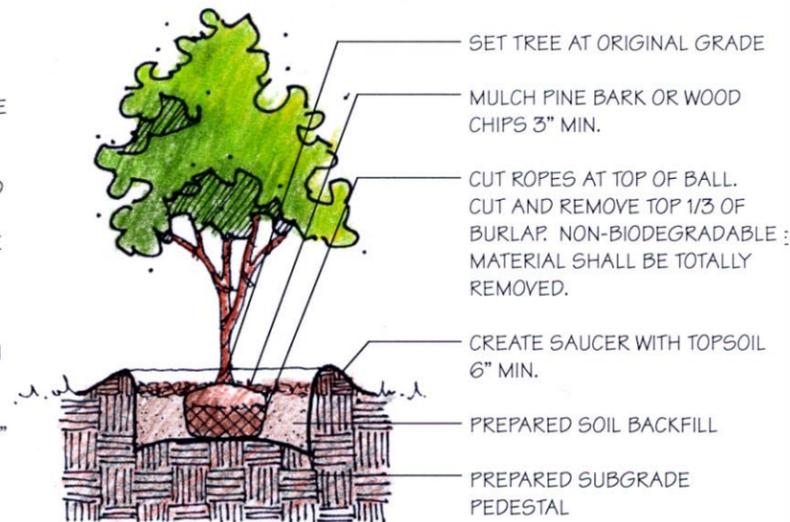
MULTISTEM TREE PLANTING



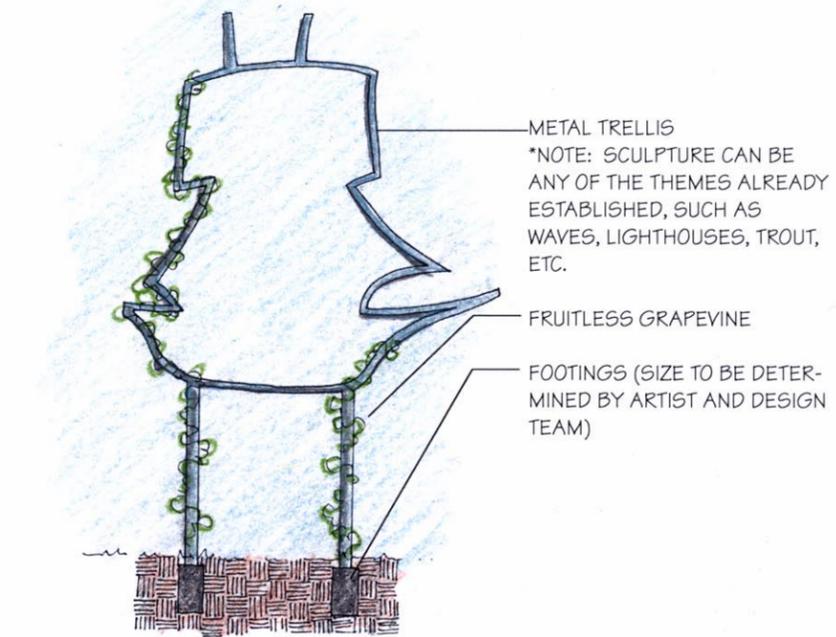
CONIFEROUS TREE PLANTING



BALL AND BURLAP SHRUB PLANTING ON SLOPE



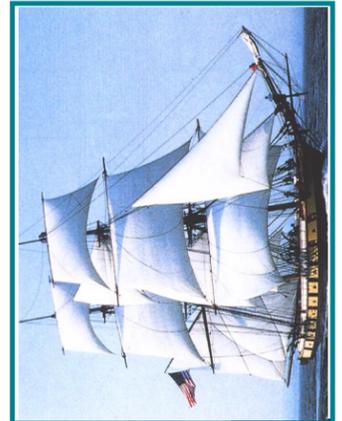
BALL AND BURLAP DECIDUOUS TREE PLANTING



DECORATIVE TRELLIS FOR VINES

# Landscape Planting Specifications

## Erie Gateways and Greenways Design Guidelines Landscape Planting Specifications



## **NEXT STEPS**

### **“EARLY WIN” CONCEPTS**

To further illustrate how these prototype elements can be applied throughout Erie, workshop participants discussed a range of possible “early win” concepts or projects. These projects represent a cross-section of the kinds of initiatives that can be achieved within the overall theme. Each project provides an opportunity to involve the community in a partnership to expand the applications of themes and context sensitive design. The projects are listed below and are shown in the chart on the following page in terms of who is responsible, what partnerships are needed and a possible timeframe for completion.

1. Wayfinding Signage
2. Gateway Projects
3. GoFish Project
4. Gorge Bridge Enhancements
5. Presque Isle Connector Trail

### **IMPLEMENTATION PROCESS**

Having the vision to begin creating Gateways and Greenways will only result in successful projects if stakeholders agree to support the implementation process. This requires the flexibility of all parties and a willingness to both experiment and cooperate. It must be made clear that this process involves some change from prior ways of doing business – but for a community looking to re-establish its identity through the development of a context-friendly infrastructure, change is part of the territory.

**Champion:** An important component of the implementation process will be the establishment of a “champion” to move projects forward. It has been suggested that the Erie Metropolitan Planning Organization (MPO) is in the best position to fulfill this role. Regional leaders participate in the Erie MPO. Also the Erie MPO has a regional perspec-

tive and participates in the development of the Erie Transportation Improvement Plan. The Erie MPO is reviewing its potential to serve as the coordinating organization that maintains continuity as projects move forward. They are in a good position to communicate with stakeholders, facilitate meetings and provide contact with transportation leadership.

The champion, whether the Erie MPO or another appropriate organization, should meet on an annual basis or with greater frequency as necessary. More importantly, it should also take this Plan and develop it into an implementation approach that further defines Erie’s themes, icons and greenways. The approach should also target specific projects and locations to incorporate themes, icons and greenway concepts. It should recommend a process to coordinate projects in target areas with various entities such as the City of Erie, surrounding townships, Erie County, the Port Authority, PENNDOT and other state agencies and involved stakeholder groups. Gateway and greenway plans should be incorporated into the project identification of the Erie Transportation Improvement Plan. In this way, future projects such as bridge or roadway reconstruction will be identified as candidates for the application of Gateways and Greenways before engineering and design begins.

**Process:** For state funded transportation-related projects, PENNDOT should work with the champion charged with incorporating Gateways and Greenways. PENNDOT’s communication with the champion and stakeholders should be early and continuous. They should identify the needs of the community. With the aid of a defined approach, PENNDOT, the champion and project stakeholders should work together to ensure that themes and icons are appropriately applied. Consensus on scope, application, and maintenance responsibility should be forged before proceeding, as well as consensus on approach and public participation. It will be very important to keep the public well in-

formed. Finally, for any gateway or greenway project, the landscape, the community and valued resources must be understood before the project proceeds.

**Adoption Program:** Based on the existing “Adopt-a-Highway” model, the potential exists to “adopt” Erie’s Gateways and Greenways to ensure that local ownership exists from the design process through ongoing maintenance. This presents an excellent opportunity for businesses, nonprofits and individuals to gain recognition and support Erie’s continued success.

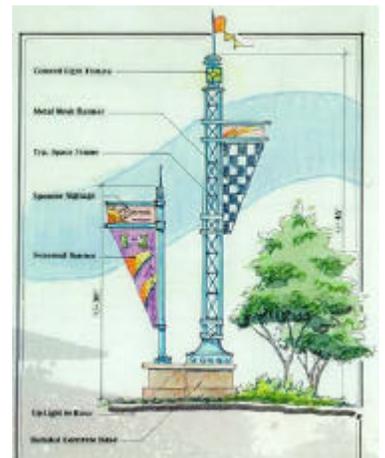
**EXAMPLES OF IMPLEMENTATION STRATEGIES:**

**Phoenix, AZ:** If you drive down Squaw Peak Parkway in Phoenix, Arizona you will see an example of Phoenix’s Public Art Program. Along the Parkway are a 20-site, 35-piece sculpture, landscape, and urban design project along a five-mile stretch. This unique “placemaking” feature of Phoenix was made possible through the foresight and adoption of a “Percent for Art Ordinance” passed by the city of Phoenix in 1986. The ordinance allocates up to one percent of the city’s Capital Improvement Program for public art projects. A public art master plan was created in 1988 that divided the city into a series of “urban design systems” and public art “working zones.”



**Figure 9 – Wall treatment with sculpture used along the Squaw Peak Parkway, Phoenix, AZ.**

**Indianapolis, IN:** “The Indianapolis Motor Speedway is proud to be a part of this unique public-private partnership. Government, businesses and private citizens have come together to improve the appearance of an important gateway to our community and bolster the economic development efforts of the Town of Speedway and the west side of Indianapolis.” This is a quote from Tony George, Indianapolis Motor Speedway President at the announcement of a \$3.1-million corridor enhancement project on the city’s northwest side. The project was made possible through a cooperative spirit that marked a joint venture between the City of Indianapolis and the Indianapolis Motor Speedway. The project entails providing a facelift to the corridor in front of the Speedway through the placement of landmark pylons, decorative banner poles, and streetlights, decorative traffic signal poles, new landscaping and the placement of brick accent paving along the street. Funding sources for the project include federal transportation enhancement funds, the State of Indiana, the City of Indianapolis, Town of Speedway and the Indianapolis Motor Speedway.



**Figure 10 – Sketch of gateway elements proposed for the corridor in front of the Indianapolis Motor Speedway**

**Early Win Concepts**

<b>WHAT?</b>	<b>WHO?</b>	<b>WHEN?</b>	<b>HOW?</b>
1. "Champion"	MPO, gathering place for leaders "Green Committee" Greater Erie Chamber	Immediately Propose to MPO board	At conclusion of report <i>Jake Welsh to confirm</i>
2. Five Gateway Projects	Universities Neighbor Associations PENNDOT Garden Clubs Sons of Lake Erie Airport Authority High School Zoo	1 year from today, complete ribbon cutting	Partner with PENNDOT Projects Airport Expansion Corporate Gifts Adopt-A-Gateway Program
3. "GoFish"	Gannon University & Arts Community 100 Business & Civic organizations locate sculptures Fish Commish	Happening right now	Need to extend this event (make the sculptures a permanent feature)
4. Wintergreen Gorge Bridge/ESAH	PENNDOT-piers, railings, walkways	Happening right now	Review of Plans
5. Connector Trail between Seaway Trail and Presque Isle	MPO-Sponsor of design PENNDOT University/High School	6 months for design 24 months for construction	MPO Funding underway
6. Wayfinding Signage	Erie Area Council of Government	Fall/winter, first phase	Underway

## **CONCLUSION**

With Lake Erie as a central theme, key icons such as sailboats, fish, waves, sunsets, lighthouses, native plant species and other nautical elements relate to both the region's history and future. Since the nautical / Lake theme includes connecting both the human and natural environments, transportation projects will play a key role. This document provides a series of prototype projects that can serve as models for longer-term efforts. A series of "early win" concepts have been identified that can be implemented in the short term. With a consistent theme, community partnership and a toolbox of ideas, Erie, PA is prepared to enhance its Gateways and Greenways. With a positive vision of the future, Erie is a Great Lake Community.

## **APPENDICES**

- A - Interviews with Stakeholders
- B - Workshop Summary
- C - Key Resources
- D - Contact List

## **APPENDIX A**

### **Interviews with Stakeholders**

#### ***OBJECTIVE***

The project team conducted interviews of PENNDOT and community leaders to better understand what they hope to accomplish by an examination of options for organizing themes, Gateways and Greenways in the Erie, PA region. Most of the interviews were conducted in Erie and Oil City during the month of April 2001.

#### ***PENNDOT - INTERVIEWS***

PENNDOT staff involved in Erie area projects were interviewed on April 6, 2001. These interviews were conducted by Annette Schultz of the RBA Group in the District 1-0 PENNDOT offices located in Oil City, PA. The following is a summary of what we heard from the PENNDOT staff. The interviewees set the stage by providing background information. They also described their expectations and objectives, challenges and needs to be met, Erie assets and icons, and opportunities for cooperation.

#### **PENNDOT provided background information and expressed the need for a workshop to address enhancements and themes:**

- The East Side Access Highway (ESAH) is a prototype. It passes through rural, suburban, and urban sections; consequently, this project can provide templates appropriate to a range of environments. There will be signalized crossings on the ESAH. A portion of the ESAH has been constructed. Other sections are in the design stages and will be constructed in phases.
- PENNDOT cooperated with partners to enhance the Bayfront Parkway by providing a strip of land from PENNDOT's right-of-way and turning over its development and management to local organizations. The SONS of Lake Erie (Save Our Native Species) landscaped the area and installed welcome signs with a nautical theme.
- PENNDOT has purchased and condemned right-of-way for city streets on the City's behalf.
- PENNDOT has cooperated with the City of Erie in Adopt-a-highway and Adopt-an-Island Programs.
- The bridge on the ESAH that has been designed to cross Wintergreen Gorge, a local natural "treasure," has been the focus of community dialogue with PENNDOT. There were two major goals:
  - The environmental perspective is concerned with minimizing environmental impacts and visual impacts, blending into the natural environment.
  - The tourism and cultural perspective is concerned with creating a visual impact that communicates Erie's distinct and unique character, making an architectural statement related to gateway themes.
- Within PENNDOT departments, there was opposition to design suggestions based on safety and liability concerns.
- A compromise was reached and an alternative selected.

- The SONS of Lake Erie gateway garden and sign on 8<sup>th</sup> St and Bayfront is an example of a successful partnership.
- There is community interest in a signing program for Erie. The Erie County Signage Committee has produced a report recommending gateway signage.
- A custom “planter wall” was designed and built by PENNDOT.
- There is not much time to affect the ESAH design as it is in the final stages.
- The Ernst Company, a garden supplier, has been partnering with Penn State to investigate options for roadside vegetation and landscaping.
- PENNDOT has historically been concerned with the safety and engineering of transportation projects. Focusing on enhancements is a relatively new direction for PENNDOT.
- PENNDOT is often limited by the amount of right-of-way they can secure.
- Citizen expectation of transportation projects has not always been aligned with the reality of implementing the projects.

### **PENNDOT staff outlined their expectations and objectives for this project that looks at themes, gateways and greenways:**

- Objective is to set the direction for ongoing community support, promote adoption of facilities, and engage business and industry participation.
- A goal is for industry to support the facilities.
- There has been some concern with the bikeway constructed in the first phase of the ESAH. The feeling is that it could incorporate more of the community’s greenway ideas and Erie character. This project should address how that can be done.
- This project should come up with themes that PENNDOT could support through design or with strategies for maintenance by others; for example, PENNDOT could include water lines for others to use to establish plantings.
- Everyone must be made aware that the recommendations will require additional money that must come through partnerships, and not depend solely on PENNDOT sources.
- Partners and their responsibilities must be identified along with each recommendation.
- A goal is to apply greenways concepts to existing highways, as well as the ESAH.
- Whet appetite for what can be done – show examples.
- The project should recommend ways to create a forum for businesses to get involved.
- The report should be an “action plan” and identify next steps.

### **PENNDOT staff also outlined their expectations and objectives for enhancing regional transportation corridors:**

- To enhance the City of Erie.
- To make the region more desirable for visitors.
- To improve public perception of Erie.
- To give back to the public and make up for neighborhood divisions created by the new ESAH.
- To help blend the highway into the surrounding neighborhoods and make it a part of them.
- To make the City green and connect neighborhoods for pedestrians.
- To revitalize neighborhoods on the decline.
- To provide visitors with an experience of an attractive and unique place – not just a bituminous ribbon that looks like every other highway.
- To provide a consistent identifying theme.
- To improve the Erie community’s perception of PENNDOT.
- To be a “master plan”, providing direction future projects of all partners.

- To stimulate economic revitalization as the Bayfront Parkway has done.
- Bike/ped facilities should relate to existing facilities and have common themes. The Seaway Trail begins in St. Lawrence and comes through Erie, several hundred miles. Erie's bike/ped facilities should say, "Welcome to PA".

### **PENNDOT described the steps that will lead to success:**

- Input from participants, followed by feedback to participants.
- Disseminate information and results widely, including all potential partners. This should be a master plan for future projects of all partners.
- Recommendations can extend to maintenance regimens. PENNDOT has been experimenting with the use of native species and other plantings that provide color or reduced water requirements.
- Maintenance crews could be trained to support facilities/plantings recommended.
- Agreements could be drafted defining tasks that partners could accomplish, and how PENNDOT would participate (example: Adopt-an-Island).
- The public is needed for implementation.
- Erie MPO may need to allocate funding for potential projects.
- The City of Erie, County, and other government entities that need to commit to accepting ownership and maintenance responsibility.
- SONS of Lake Erie and other citizen groups need to participate.

### **PENNDOT staff described some of the needs and challenges associated with enhancing transportation projects:**

- Signs for tourists directing them to attractions.
- Uniform and consistent signage.
- No crown vetch on fill.
- Shady areas, with places to stop and rest.
- Planting diversity – not just grasses, but trees and "nature".
- Color is needed.
- Educational signs for children and adults, highlighting Erie's unique animals, relationship to the canal, and history.
- There are historical structures on the ESAH that can be used to highlight history (see EIS). There are plans for a plaque that is still in design.
- Architectural elements should be enhanced.
- Sound barrier walls can have texture and color. PENNDOT is working with PHMC to design some for the ESAH.
- Lighting should be distinctive.
- Retaining walls and bridge structures should be carefully reviewed.
- Artwork can be incorporated. Example: a speakeasy in Pittsburgh maintains a mural on a retaining wall at a stoplight.
- The right mix of plants is needed.
- PENNDOT can implement, but needs partners.
- Costs and funding are always a challenge.
- Satisfying safety requirements cannot be overlooked.
- Consensus is important.
- The ESAH should complement an existing pedestrian bridge.

**PENNDOT staff listed what they perceived to be the assets, attractions, and icons of Erie:**

- Maritime (museum)
- Flagship Niagara
- Lighthouse (not usually associated with Erie or widely recognized)
- Presque Isle
- Driven, focused local leadership

**PENNDOT participants gave their views on opportunities for partnering to maintain transportation enhancement projects:**

- The Albion prison system has craft people who might participate.
- DCNR and the Western PA Conservancy have a program to encourage “signature” flower displays in towns and cities.

## ***COMMUNITY LEADERS - INTERVIEWS***

Community leaders of the Erie, PA region were interviewed on April 26, 2001. These interviews were conducted by Lisa Byers, of the RBA Group, in the offices of the leaders. Representatives from the following organizations were interviewed:

- Erie Western PA Port Authority
- City of Erie, Planning Department
- County of Erie, Planning Department
- Bayfront Access & Beautification Organization
- Erie Conference on Community Development
- State Transportation Commission
- SONS of Lake Erie
- Neighborhood Watch

Overall, this group of community leaders thought Erie was a great place to live and work, as well as a great place to visit. They have been described as committed local leaders, driven to make Erie livable and attractive. Provided are their responses to questions regarding their objectives and expectations, local and national precedents, challenges and needs to be addressed, current plans, local assets, and opportunities for cooperation.

**The community leaders outlined their objectives and expectations for enhancing the regional transportation corridors:**

- Enhance Erie’s internal self-image.
- Provide access to Erie’s amenities.
- Share the Bayfront with visitors, bring visitors into Erie.
- Soften the harsh image of typical highways.
- Enhance transportation projects that are under development.
- Ideas or themes from the transportation enhancements could spill over into public buildings (use of themes in public buildings).
- Important to incorporate natural amenities and landscapes.
- Important to use bridge structures as gateways, where appropriate.
- Erie could be the prototype for transportation corridor enhancements that could be used elsewhere in the state.

**They described a process for achieving results:**

- Develop a plan and put it into action.
- Obtain funding.
- Obtain support from neighborhood groups.
- Work towards cooperation with PENNDOT.

**The leaders were asked if they were aware of any local or national precedents for enhancement of transportation projects. Locally they noted:**

- Negotiated the Bayfront Parkway right-of-way so that the trail could meander along the parkway, with a buffer between the two.
- The SONS of Lake Erie have done a nice job installing plantings around the entrance signage of the Bayfront.

**Nationally they described several precedents:**

- Chicago's waterfront
- Louisville's waterfront
- Tempe/Phoenix area has colorful stone embankments along the roadside and sound walls
- Bluebird trails
- Disneyway (?) Boulevard
- Pittsburgh's linear park along the waterfront
- George Washington Parkway in Northern Virginia: it provides a very different experience entering Washington, D.C. than other highways in the area.

**They outlined some of their regional and organizational challenges and needs:**

- The region needs a vision for 2020, especially related to planning, travel & tourism.
- The existing Council of Government (COG) needs to be stronger and more involved in planning and economic development (tourism).
- The COG is responsible for area signage.
- County, city, and townships need to work together.
- Nonprofit groups are doing what politicians should do.
- The MPO (Erie County Planning Department) must be the leader. They must act as "the champion."
- There is a need for an administrative entity to oversee the community groups.
- Key partners in any Erie area transportation enhancements are: Environmental Coalition, neighborhood watches, garden clubs, and business partners.

**They outlined specific challenges and needs associated with area highways and roads:**

- Improve the on road experience.
- Incorporate the "feel" or the character of Erie.
- Roadways should look less engineered and more landscaped.
- Observe and use beach vegetation, where appropriate.
- Right now people traveling on the interstates, especially I-90 never know what they just passed through or by.
- It is important to "think beyond the curb."
- There is a need for landscaping, landscaping, landscaping.
- Signage is important.

- Peachtree Corridor needs mitigation. It is a strip area of big box stores, wide road, and many power lines. No pedestrian opportunities. Needs uniformity and smaller scale elements.
- The main corridors need a theme, continuity, but not necessarily a uniform identification.
- It is important to develop or enhance the transportation corridors.
- Roads and highways should be seen in a bigger perspective rather than just “curb to curb.”
- Scenic overlooks should be added as a feature to some of the area roads.
- Views of the lake should be celebrated.
- I-79 northbound travels through a suburban and then an urban area. This should be recognized somehow on the highway.
- Enhancements should be more than just “screening out ugly areas.”
- At intersections and their corners, these corners need to be strategically designed and they must be adopted by neighborhoods. Neighborhood signage could be placed at these corners.
- Take advantage of natural drainage.
- Use native, low maintenance plantings.
- Trees and roadside vegetation should be similar to what can be found in the surrounding neighborhoods (for instance, pine trees may be foreign to neighborhood, but not oak trees).
- Where appropriate, trails of 12 foot width are desired over 8 foot or 10 foot wide trails.
- Rolling type of mounds should be placed between a roadway and a trail.
- It is important to test the soil before planting trees.
- Retaining and sound walls could be camouflaged with murals, though may attract graffiti.
- It is important to consider all FOUR seasons in the design of projects.
- Current roadside development results in monotony.
- It is important to brighten the highways, provide freshness and greenness.
- Good directional signage is needed throughout the area.
- Use signage to identify the Lake Erie watershed.

### **They discussed key intersections or entrance points that could be developed as a gateway or enhanced:**

- Draw people off I-90 and into Erie. I-90 entrances to Erie.
- I-79 and 26<sup>th</sup> Street as a gateway.
- Bayfront Parkway and 8<sup>th</sup> Street, on west side near Frontier Park.
- Entrances to the Bayfront should be framed. Entering the Bayfront from the west there is a plain bridge abutment that could become more of an entrance.
- The entrance to the Bayfront via I-79 at 6<sup>th</sup> Street is an important demarcation. There is not even a sign here.
- Banners could enhance some of the intersections and gateways.
- New construction at the Zoo, 38<sup>th</sup> Street and Route 505.

The East Side Access Highway is a new limited access highway that will transverse the east side of the Erie, from Exit 9 of Interstate 90 to the Bayfront Parkway. One section of the highway is already built and the remaining sections are currently in design or final design stages. **The community leaders expressed expectations and concerns about the impact, character and details of the new highway:**

- ESAH can be considered as having three different sections or environments: from I-90 through Behrends College to Wintergreen Gorge– characterized by open and wooded areas); from Wintergreen Gorge to 38<sup>th</sup> Street– characterized as suburban and light industrial); 38<sup>th</sup> Street to Bayfront– characterized as urban.

- It is important to understand what is planned for the ESAH.
- This road through the city shouldn't look like a "highway."
- The ESAH needs to be "greened."
- Elements of the "districts" the highway passes through should be incorporated into the highway design.
- Hide or mitigate Liberty Iron & Metal, where the two main rail lines intersect. Maybe distract from undesirable views with a piece of sculpture.
- The proposed concrete sound walls along the highway will severely divide neighborhoods and are not necessarily needed.
- PENNDOT presented area residents with options for concrete sound walls. Residents voted on the color of the walls.
- Provide some variation in the plantings along the highway.
- "Green" the median strips. Avoid concrete jersey barriers as medians wherever possible.
- Some community leaders are uncertain about what type of vegetation will be planted in the planters between the highway and the rail line.
- For the section of the ESAH that has already been constructed, the trail is too close to the roadway at times creating a difficult situation for snow removal (snow is pushed onto the trail).
- The trail along the highway should be separated from the roadway by vegetation, median, berm, or bollards to provide a safety buffer and to provide a place to put the snow so it does not pile up on the trail.
- Bluebird boxes could be placed along the highway.
- The highway should be carefully lighted.
- Benches could be provided at strategic locations along the highway.
- The property being condemned along the highway and given to the City could be loaned or rented out by city residents for flower or vegetable gardens.

### **They also expressed reflective concerns about the existing Bayfront Parkway:**

- The road unfortunately looks too "highway."
- Bayfront needs a gateway or entrance marker.
- Need to get the townspeople to Bayfront and its trails. Need at-grade crossings.
- The seawalls along the parkway are not necessarily needed and they block the view.
- The sewerage treatment plant along the Bayfront needs to be screened.

### **The community leaders discussed other transportation-related projects that are on the drawing boards. Regional projects include:**

- Feasibility study on several trails in the County – rail with trail from Behrend College to Union City to Corry to connect with trail from Meadville; rail-trail from Corry to NY line, connection with a trail coming from Titusville; and trail from Girard to Crawford County on old Bessemer rail line.
- Planned is a connection of Asbury Park with Presque Isle via Route 832 and a greenway.

### **Enhancements planned for the Bayfront include:**

- New Convention Center (& hotel?)
- Restaurants, retail and apartments in a new "old village" at the waters edge
- Overhead walkways
- Miniature golf course

- Camping facilities
- More short term and long term boating slips
- Ferry service to Presque Isle
- On the south side of the Bayfront Parkway, maintain the bluff as mostly natural (challenged by sewer line passing through this area)
- Provide seating on the bluff
- Considering planting a vineyard on the bluff
- Proposal for a bird aviary on the south side
- Proposed office building on the western end of the south side of the Parkway; elevator in building will transport people from bluff to overhead walkways or street level.

**The objectives of Bayfront enhancements are:**

- Increase commerce (port related)
- Enable dry dock ship building
- Attract tourists – dining, retail, lodging
- Retain tourists - if they have a good experience, they will come back

**The community leaders very proudly listed assets, attractions, and icons of the Erie region:**

- Lake Erie
- Presque Isle Bay
- Presque Isle
- Sunsets
- Nature preserve on Presque Isle
- Birds
- Wildlife
- Nurseries
- Vineyards & Wineries
- Seaway Trail
- Beaches on Presque Isle
- Bayfront
- Fishing
- Winter Activities
- Zoo at Glenwood Park
- Flagship Niagara
- Lighthouses
- Russian Church along the Bayfront
- Historic avenues
- 26<sup>th</sup> Street
- Neighborhood Revitalization
- Erie is a big town with a little town attitude
- Erie has a variety of entertainment
- Erie is inexpensive
- Erie is clean

**More specifically, amenities of the Bayfront were listed:**

- Dobbins Landing – Bicentennial Tower
- Liberty Park – open air amphitheater
- Library and Maritime Museum
- Flagship Niagara
- Lighthouse
- Waterworks Buildings (Block House?)
- Trail along Bayfront Parkway
- Benches along water
- Restaurants
- Marinas
- Large ship terminals
- Shipyard, ship building

**Also delineated were winter attractions:**

- Ice fishing
- Ice boating

- Snowmobiling
- Winter Carnival
- I-90 splits the snow belt (snows more north of highway than south)

**The community leaders also outlined their views on opportunities for partnering to maintain transportation enhancement projects:**

- Neighborhood groups need to adopt the proposed planters along the ESAH.
- Plantings between a roadway and a trail must be carefully considered for ease and efficiency of maintenance. For example, grass should be able to be cut in relatively straight passes, without needing extra passes or around extra curves.
- There are youth programs that require community service. Maintaining roadside landscape could be a community service project.
- PENNDOT mostly engages in contract management, foresee same operation.

***LIST OF INTERVIEWEES***

***PENNDOT Staff – District 1-0 Offices, Oil City, PA***

John Baker, District Engineer  
Michele Dumbleton, Project Liaison  
Robert Fromknecht, ESAH Liaison Engineer  
Karl Ishman, Assistant District Engineer (phone interview)  
Mike McMullen, District 1-0 Liaison Engineer  
Tom Minnich, Environmental Manager  
Deborah Schreckengost, Press Secretary  
Erin Wiley, Environmental Planner  
Brian Yedinak, Design Services Engineer

***Community Leaders – Erie, PA***

*Erie Western PA Port Authority:*

Ray Schreckengost  
Tom Maggio

*Bayfront Access & Beautification Organization:*

Jeff Gault

*State Transportation Commission:*

Sharon Knoll

*Erie Conference on Community Development:*

Jake Rouch

*City of Erie:*

Jeff Spaulding  
Mark Kukla

*S.O.N.S. of Lake Erie:*

Edward Kissel

*County of Erie:*

David Skellie  
Jake Welsh

*Neighborhood Watch:*

Edna Schwenk

## **APPENDIX B**

### **Workshop Summary**

A Gateways and Greenways Workshop was held on May 10, 2001 at the PSU Behrends College, Erie, PA. Invitees included community leaders in and around Erie and PENNDOT District 1-0 officials. John L. Baker, District Engineer, PENNDOT District 1-0, delivered the opening remarks. Following the remarks, introductions were made. The Project Consulting Team (Team) was introduced and each participant introduced himself or herself.

The Team outlined the purpose of the workshop– to “brainstorm” together:

- Identify unifying themes to guide Erie’s transportation and gateway enhancements.
- Identify strategies to apply those themes to visitor sites and transportation corridors.
- Identify ways that PENNDOT and the Erie community can work together to implement theme-based improvements.

Karl Ishman, Assistant District, PENNDOT District 1-0, gave an overview of PENNDOT activities in the Erie region. He was followed by several participants who outlined other exciting projects occurring in the area: the Erie Western PA Port Authority overseeing projects on the Bayfront, the Erie Metro Transit Association expansion and related projects, and the Erie Zoo expansion and related projects.

The Team next presented ideas and examples from other communities on themes, gateways, and greenways. They emphasized that a grand entrance is important and that money spent on giving a community a “sense of place” will have a greater return on investment than any possible Stock Market investment. They noted that much of the United States has a mono-infrastructure– supporting only motorized vehicles. The other choices of transportation are not given much emphasis and the general public is not often invited to get involved in making decisions. The Team suggested transportation connections should be about people. Finally, they noted that a consistent design ideology is needed for the Erie region.

Regarding project themes, the Team challenged the participants to think about:

- What themes and icons symbolize Erie?
- What can serve to unify projects in the Erie region?
- What are some of the desired roadway treatments and design considerations?
- How do you want Erie to be perceived relative to your Bayfront/Lake/Interior/Recreation attractions?

To facilitate discussion on themes, the participants were divided into four groups focusing on the following areas of attraction: Bayfront, Lake, Interior (non-lake or Bayfront), and Recreation and Trails. As a guide, they were given the following questions to answer in their discussions:

- What important visitor sites and attractions are in the Erie region?
- What images, icons, or symbols characterize these attractions?
- How are the sites “related” to each other?
- What central organizing ideas or themes could define or apply to these sites?
- What are some examples of ways that a theme can be applied to each site?

The following pages contain a chart of each groups’ discussions.

## **BAYFRONT THEME - DISCUSSION**

### Visitor Sites & Attractions

Lake  
Frontier Park  
Sunsets  
Arboretum  
Visitors Center  
Park & Ride  
Amphitheater  
Bluffs  
Waterworks Buildings  
Marinas - Tour boats/cruises/boat launch,  
camping  
GAF Building  
Strong Vincent  
Bayfront Bikeway  
Library  
Maritime Museum  
Bicentennial Tower - Dobbins Landing  
Convention Center

Intermodal Center - Ferry Boat Service  
Brig Niagara/Center for Maritime Studies  
Restaurants/Shops  
Metro Machining/Erie S & G Mounfort Terminal  
Wastewater treatment  
Wayne Blockhouse  
South Pier  
Medical Center (Hamot)  
Seaway Trail  
Railroads  
Wolverine Moorings  
Water gateways  
Erie Yacht Club  
Channel entrance  
Shipping at Port Erie  
Sommerheim  
Rowing/crew  
(Some attractions are seasonal)

### Special Events - Attractions

Jazz Festival  
Winter carnival  
Tall ships  
Fishing  
Regatta/speed boats

Concerts  
Marine Museum events  
Bike/foot races  
Erie-z days  
Tour boats/water taxi/dinner cruises

### Images/Icons/Symbols

Nautical Flags  
Brig Niagara  
Bicentennial Tower  
Arboretum  
Views of Perry Monument  
    Gold Church Dome  
    Library  
    Waterworks Buildings  
Seaway Trail Symbol  
Dry Dock Building/Metro Machine Shipyard

Anchors/Wood Pilings  
Liberty Park  
Sunsets  
Wayne Block house  
Erie Coke plant  
Bluff  
People Fishing  
Boating/Rowing  
(Port Authority is developing a logo)

### Relationships – Attractions/Sites

Water/highways/trails  
Natural environment  
All within city of Erie  
People  
Entertainment

Activities:  
    Tourism/Vacations  
    Fishing/Boating  
    Cultural/Educational  
    Winter activities  
    Business

### Central Organizing Ideas

Brig Niagara  
Anchor  
Land lighthouse  
Wayne Blockhouse  
“Rising” Bayfront

Flags - spelling ERIE  
Water/Land - Wayfinding Signage theme  
Port Authority logo  
Flagship City

**BAYFRONT THEME - DISCUSSION, continued**

Application to Sites

Bayfront Parkway has helped to organize attractions, also promenade  
 Uniform design/colors  
 Different colored bikeways  
 Standard signing program  
 Landscaped medians  
 Incorporate in architectural  
 Buildings/improvements (uniform themes)

Common landscape features  
 Period lighting - in pedestrian bridge design  
 "GoFish" sculptures  
 Murals/ Public Art – For Example:  
 Bayfront Promenade - Plates/Granite Seating/Seating areas with informed plaque  
 Apply themes to all transportation modes  
 Maintenance

**LAKE THEME - DISCUSSION**

Visitor Sites & Attractions

Lake Erie & its watershed  
 Light House  
 Fishing  
 Boating  
 Swimming - Beaches  
 Bird Watching

Camping  
 Photography  
 View Sunsets  
 Ice Skating  
 X-Country Skiing  
 Jogging, biking, roller blading

Special Events - Attractions

Wine Festival  
 Cherry Festival  
 Discover Presque Isle Days  
 Fishing Tournament

Cascade Creek & Ponds  
 Fish Stocking  
 Winter Festival  
 Charity Marathons

Images/Icons/Symbols

Sunsets  
 Fish/Fishing  
 Steelhead (Trout) Fish  
 – "GoFish" Sculptures

Lighthouse  
 Boating  
 Lake

Relationships – Attractions/Sites

Access to Nature  
 Relationship of access points to nature  
 Access to History

Central Organizing Ideas

Access to Nature  
 Access to History

Access Points to Nature

Bluffs  
 McDaniels Run  
 Wineries (Vinyards)  
 Shades Beach  
 Free Port  
 Foot of East Avenue

Presques Isle (7 miles of beaches)  
 Waldermeer  
 Walnut Creek  
 Elk Creek  
 Trout Run  
 Raccoon Park

Application to Sites – Design Criteria

Wayfinding Signage  
 Enhanced view of the Lake (additional opportunities)  
 Entrances – Repeatable themes with multi-modal access  
 Continuity of access points connections from bayfront area  
 Fish Sculptures— as identifiers



## **INTERIOR ATTRACTIONS THEME - DISCUSSION**

### Visitor Sites & Attractions

Bayfront (weather permitting)	Lovell Complex (restaurants, etc.)
Zoo – new leopard	Library
Glenwood Park	Perry Square
Downtown	Veteran's Stadium
Surrounding small town centers	Academy
Warner Theatre	Erie Cemetery
Baseball	Knowledge Park
Civic Arena	Asbury Woods
Discovery Square	Family-First Sports Park
Gannon College	3 covered bridges

### Special Events - Attractions

Zoo Boo (Halloween)  
Zoo Easter Egg Roll

### Images/Icons/Symbols

Zoo: Currently uses – circle with lion, flower, reptile and bird  
Elephant with light (Note: no elephants at Erie Zoo)  
Zoo is looking for a larger animal, like the elephant, to be a major attraction  
Species survival at zoo is important  
Baby leopard was recently born at the zoo, well advertised  
Zoo Card (Zoo is owned by the City of Erie)  
“Zoo” buses have tiger or zebra strips or other animal theme  
Peninsula Symbol (Erie Guideways)

### Relationships – Attractions/Sites - Transportation

### Central Organizing Ideas - Public Art (temporary) – “GoFish”

### Application to Sites

“Life” needs to be put back into the downtown  
Wayfinding Signage, blue signs near the water, green signs near the land  
Transportation  
EMTA buses  
Funding Access to \$  
Land swap for plaza with zoo  
How to pursue people to use buses  
Specific target of groups/family for activities  
Incorporate themes into transportation system  
Important for entryways to be enhanced  
More user-friendly corridors into the city  
Gateways to the attractions and the city must be able to withstand "Erie" winters  
a. How will landscaping be done?  
b. Possibility of using "colored rock" against slope – seasonal flowers poke through rocks  
How to Connect System  
Use Erie Area Council of Governments  
Wayfinding Signage being developed (blues/green used)  
Sign faces are being sponsored by Hospitals, Businesses, etc.  
Connects corridors/themes  
Incorporate Icons on state maps  
Is the Zoo really “downtown,” consider limits

## **RECREATION & TRAILS THEME - DISCUSSION**

### Visitor Sites & Attractions

State Street  
Bayfront  
Wine - vineyards  
Marshlands (wetlands)  
Rural Amish  
Streams  
Lake Erie spinoffs (all)  
Fruit/wine (NE)  
Agricultural (SW)– similar to Lancaster, PA  
Streams– fishing, kayaking  
19<sup>th</sup> Street Rails  
Green Corridor Trails (Mall)  
Cherry Street Extension

### Special Events - Attractions

### Images/Icons/Symbols

### Relationships – Attractions/Sites

### Central Organizing Ideas

### Application to Sites

Leave Erie by way of inner city gateways – transitional  
It is important to attract people off the interstate highways  
There are a multiplicity of themes within the county  
Wayfinding signage is needed to the trailheads of greenways  
On road access to off-road trails is needed  
Airport as a gateway to the city – does have EMTA (public transportation) to downtown  
Rest/Lodging - Transit partner options?!  
Visitors should be “invited” into the downtown (inviting gateways)  
Address how do you get from hotel to Presque Isle  
Sidewalks are needed from Presque Isle to downtown  
EMTA (Erie Metro Transit Authority)  
Information stops (without driving)  
Accommodate bikes on buses  
Provide information on trails & streams  
Bike Rental Center

## Gateways and Greenways in Erie, Pennsylvania a Great Lake Community

After lunch, Context Sensitive Design was discussed. The Team offered that transportation corridors can be viewed as catalysts for strengthening community life. Context sensitive design is more than just designing roads to accommodate motor vehicles; instead it aims to balance all the users of a street– pedestrians, transit riders, motorists and bicyclists, and to focus on how the road itself connects to surrounding areas and public spaces.

The participants were next asked to divide again into discussion groups and they were charged with answering the following questions– “What? So What? and, Now What?”– on the themes and projects they had identified in the morning. In their discussion groups, they completed charts on their different areas of attraction: Lake, Bayfront, Interior, and Recreation and Trails. These charts are attached.

### BAYFRONT THEME APPLICATION

WHAT?	WHO?	WHEN?	HOW?
<p><b><u>Nautical</u></b></p> <ul style="list-style-type: none"> <li>▪ Ship</li> <li>▪ Anchor</li> <li>▪ Piling</li> <li>▪ Waterfront</li> <li>▪ Sails</li> <li>▪ Wheel</li> <li>▪ Compass</li> <li>▪ Canon</li> </ul> <p><b><u>Fish/Fishing</u></b></p> <ul style="list-style-type: none"> <li>▪ GoFish</li> <li>▪ Steelhead Trout</li> <li>▪ Walleye</li> <li>▪ Perch</li> <li>▪ Yellow Perch</li> </ul> <p><b><u>Heritage/History</u></b></p> <ul style="list-style-type: none"> <li>▪ Blockhouse</li> <li>▪ RR/Shipping</li> <li>▪ Museums</li> <li>▪ Church Dome</li> </ul>	<p>Local &amp; State Government</p> <p>Port Authority</p> <p>Bay Area Beautification Org</p> <p>Erie Conference</p> <p>Corporations Businesses</p> <p>Civic Organizations Garden Clubs</p> <p>Community Groups Convention &amp; Tourism Bureau</p> <p>Historical Society, etc.</p> <p>Convention Center Authority</p>	<p><b><u>Sooner</u></b></p> <ul style="list-style-type: none"> <li>▪ Ongoing Wayfinding Signage program</li> <li>▪ GoFish</li> <li>▪ Landscaping</li> <li>▪ Bluff Design Project</li> </ul> <p><b><u>Later</u></b></p> <ul style="list-style-type: none"> <li>▪ I-79 Gateway</li> <li>▪ Ordinances for the future</li> <li>▪ ESA Portals (Bayfront)</li> </ul>	<p>Government o Private i Industry i Public</p> <p>Adoption of Thematic Ordinance</p> <p>Media Exposure</p> <p>Public Education *Schools (student participation)</p> <p>Maintenance through education partnership</p>

**LAKE THEME APPLICATION**

<b>WHAT?</b>	<b>WHO?</b>	<b>WHEN?</b>	<b>HOW?</b>
<ul style="list-style-type: none"> <li>—</li> <li>▪ Enhanced Lake Access</li> <li>▪ Trail Access: Seaway Trail - from Presque Isle entrance to Lake</li> <li>—</li> <li>▪ Watershed identification on I79/I90</li> <li>—</li> <li>▪ Market Seaway Trail</li> <li>▪ Provide Scenic Overlooks</li> </ul>	<p>Find and fund a <b><i>Champion</i></b> who will take the lead Community</p> <ul style="list-style-type: none"> <li>—</li> <li>▪ Townships</li> <li>▪ PENNDOT</li> <li>▪ Sportsman Organizations</li> <li>▪ County Office</li> <li>▪ Environmental Groups/DEP</li> <li>▪ DCNR</li> <li>▪ Private Sector</li> <li>▪ Local C.O.G.</li> <li>—</li> <li>▪ L. E. R. C.</li> <li>▪ Western Pennsylvania Conservancy</li> <li>▪ PENNDOT</li> <li>—</li> </ul>	<p>Start Immediately and Work in Phases</p>	<ul style="list-style-type: none"> <li>▪ Engage Private Sector</li> <li>▪ Achieve small successes to draw in others</li> <li>▪ Develop “Competition”</li> </ul>

**INTERIOR ATTRACTIONS THEME APPLICATION**

<b>WHAT?</b>	<b>WHO?</b>	<b>WHEN?</b>	<b>HOW?</b>
1. Countywide Wayfinding Signage System	C.O.G. PENNDOT	Begins next year	- Grants in place - County Money
2. Zoo Expansion - Master Plan	Zoo Board Community City of Erie PENNDOT	Starts with road realignment -1-2 years	Enhancement Funds (PENNDOT) Private Money for match funds
3. Parking/Movement Study	EMTA Zoo Board City of Erie	Start Now	PENNDOT City of Erie Zoo Board (study)
4. North/South Study of Movement	C.O.G. PENNDOT Developers	Start Now	Grants Private Money Required Developers

**RECREATION & TRAILS THEME APPLICATION**

<b>WHAT?</b>	<b>WHO?</b>	<b>WHEN?</b>	<b>HOW?</b>
1. Trail Systems - connecting on and off road (whole network) — 2. Gateway – Two Way System – Entrance & Exit – Address diversity of county. i.e., Bayfront NE Vineyards, Agriculture, Forest Areas — 3. Inner-Connecting Modes (Buses with bike racks/give a choice of mode/take advantage of mass transit)	Multi-Agency Public/Private Partnership  Sponsorship - Adopt A Gateway  — Erie MPO  — ¾ Business & Industry Civic Groups – Garden Clubs Kiwanis Scouts Schools Non-Profits such as Trout Clubs	—  — Incrementally & Phased  — Painting lines  Trail System enhanced immediately  — Beautification	—  — Partnerships  Local Leadership  — Expansion of Existing Structure  Clarification of Roles & Responsibilities  Adherence to Minimize Standard

Each group presented its findings. Following the presentations, the Team outlined what we had heard during the day – reoccurring themes and their application. A motto was proposed: “Erie, a *Great Lake Community*.” The Team stressed the importance of finding a “champion” to organize the implementation of theme-based projects and applications. The “champion” cannot be PENNDOT, since they are physically located outside Erie and are engaged in state-related transportation activities only. A likely champion suggested by participants is the Erie Metropolitan Planning Organization (MPO). They are a gathering place of leadership and community groups in the Erie region and are involved in transportation decision-making.

The Team also suggested several gateway/theme-related projects that are underway or can get underway in the very near future. Some of the projects are proposals or studies that were mentioned during the workshop. Suggested projects are listed below in the chart format previously used.

**SUGGESTED NEAR-TERM PROJECTS**

<b>WHAT?</b>	<b>WHO?</b>	<b>WHEN?</b>	<b>HOW?</b>
1. "Champion"	MPO, gathering place for leaders "Green Committee" Greater Erie Chamber	Immediately Propose to MPO board	At conclusion of report <i>Jake Welsh to confirm</i>
2. Five Gateway Projects	Universities Neighbor Associations PENNDOT Garden Clubs Sons of Lake Erie Airport Authority High School Zoo	1 year from today, complete ribbon cutting	Partner with PENNDOT Projects Airport Expansion Corporate Gifts Adopt-A-Gateway Program
3. "GoFish"	Gannon University & Arts Community 100 Business & Civic organizations locate sculptures Fish Commish	Happening right now	Need to extend this event (make the sculptures a permanent feature)
4. Wintergreen Gorge Bridge	PENNDOT-piers, railings, walkways	Happening right now	Review of Plans
5. Connector Trail between Seaway Trail and Presque Isle	MPO-Sponsor of design PENNDOT University/High School	6 months for design 24 months for construction	MPO Funding underway
6. Wayfinding Signage	Erie Area C.O.G.	Late summer, first phase	Underway

Participants offered a few follow-up comments. Ecology Groups in the local high schools can help with some of these projects, especially regarding long-term care and maintenance. Also the community service requirement of some high schools can be directed towards gateways and greenways. They also noted it will be important to review the interconnectivity of the various transportation corridors, gateways, greenways and vistas.

The Team discussed their plans for a follow-up report to be completed and distributed in June and July. An important component of the report will be sketches of prototype examples of gateways and greenways for the Erie region. The report will also document the findings from the Workshop. The meeting was then adjourned.



## APPENDIX C

### Key Resources

#### **PUBLICATIONS**

Flink, Olka, Searns Rails-to-Trails Conservancy. ***Trails for the Twenty-First Century, Second Edition.*** Island Press, Washington, DC, 2001.

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Scenic America. ***Getting It Right In the Right-of-Way: Citizen Participation in Context-Sensitive Highway Design.*** Scenic America, Washington, DC, 2000.

#### **ORGANIZATIONS**

**AASHTO**  
444 North Capitol Street, NW  
Suite 249  
Washington, DC 20001  
(202) 624-5800  
[www.aashto.org](http://www.aashto.org)

**National Transportation Enhancements Clearinghouse**  
c/o Rails-to-Trails Conservancy  
1100 Seventeenth Street, NW, 10<sup>th</sup> Floor  
Washington, DC 20036  
(888) 388-NTEC  
[www.enhancements.org](http://www.enhancements.org)

**Office of Travel and Tourism**  
PA Department of Community and Economic Development  
Forum Building, Room 400  
Harrisburg, PA 17120  
(717) 787-5453

**Project for Public Spaces, Inc.**  
153 Waverly Place  
New York, NY 10014  
(212) 620-5660  
[www.pps.org](http://www.pps.org)

**Scenic America**  
801 Pennsylvania Avenue, SE  
Suite 300  
Washington, DC 20003  
(202) 543-6200  
[www.scenic.org](http://www.scenic.org)

**Seaway Trail, Inc.**  
109 Barracks Drive  
Sackets Harbor, NY 13685  
(315) 646-1000  
[www.seawaytrail.com](http://www.seawaytrail.com)

**IMPLEMENTATION EXAMPLES**

**CONCRETE TEXTURES IN NOISE CONTROL WALLS**

Pima Freeway  
Scottsdale, AZ

[www.scottssystem.com/brosum.htm](http://www.scottssystem.com/brosum.htm)

**Arizona Department of Transportation**

Simeon Brubaker, Resident Engineer  
(602) 371-1100

**City of Scottsdale**

Ron Papsdorf, Freeway Liaison  
(602) 994-7077



***Figure 11 – Creative textures and icons used on architectural concrete retaining wall, Pima Freeway, Scottsdale, AZ***

**PUBLIC ART – WALL CYCLE TO OCOTILLO**

Squaw Peak Parkway  
Phoenix, AZ

[www.harriesheder.com/oco/htm](http://www.harriesheder.com/oco/htm)

**Public Art Program**

Phoenix Arts Commission  
200 W. Washington St., 10th Floor  
Phoenix, AZ 85003  
(602) 262-4637

**PUBLIC ART – ENGLAND**

**ARTWORKS**

National Cycle Network

Outdoor public art – spread across thousands of miles of England

[www.sustrans.org.uk/ncn\\_main7.htm](http://www.sustrans.org.uk/ncn_main7.htm)

**Sustrans Head Office**

35 King Street  
Bristol BS1 4DZ, England  
Tel: 0117 926 8893

**BEAUTIFICATION/GATEWAY PLAN**

Hulman Memorial Way  
Indianapolis, IN

[www.brickyard.com/press/hulman-121699.html](http://www.brickyard.com/press/hulman-121699.html)

**City of Indianapolis / Marion County**

200 East Washington Street  
City-County Building  
Indianapolis, IN 46204  
(317) 327-4MAC



***Figure 12 – Sentinel I, Sculpture on the Sustrans Cycle Path and Sculpture Trail: Bristol and Bath, England***

## APPENDIX D

### Participation List

Interview, Workshop

**Baker, John**  
**PENNDOT, District Engineer**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7015

Workshop

**Bondi, P.E., Robert**  
**Michael Baker Jr., Inc.**  
Airport Office Park, Bldg 3  
420 Rouser Street  
Coraopolis, PA 15108  
(412)269-4600

Workshop

**Button, Don**  
**Erie Airport Authority**  
4411 West 12th Street  
Erie, PA 16505

Workshop

**Camillo, Carm**  
**Office of Senator Jane Earl**  
200 W. 11th Street  
Erie, PA 16501  
(814) 453-2515

Workshop

**Daubenspeck, Bonny**  
**Turnpike Commission**  
4921 Tramarlac Lane  
Erie, PA 16505

Interview, Workshop

**Dumbleton, Michele**  
**PENNDOT, Project Liaison**  
255 Elm Street  
Oil City, PA 16301  
(814)678-7080

Workshop

**Evans, AIA John**  
**Evans Strickland Best Architects, LLP**  
900 State Street, Suite 100  
Erie, PA 16501-1425  
(814)454-6379

Interview, Workshop

**Fromknecht, Robert**  
**PENNDOT, ESAH Liaison Engineer**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7083

Interview, Workshop

**Gault, Jeffrey**  
**Bayfront Access & Beautification**  
**Organization**  
2701 Evanston Ave, Suite 200  
Erie, PA 16506  
(814) 833-9801

Workshop

**Hanson, Mariah**  
**PENNDOT, Project Manager-ESAH A70**  
**Section**  
255 Elm Street  
Oil City, PA 16301

Interview, Workshop

**Ishman, Karl**  
**PENNDOT, Assistant District Engineer -**  
**Design**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7130

Workshop

**Karr, Jeffrey**  
**PENNDOT, Erie County Maintenance**  
**Manager**  
9031 Peach Street  
Waterford, PA 16441  
(814) 871-4411

Workshop  
**King, Shirley**  
**Summit Township**  
Summit Township Building  
8900 Old French Road  
Erie, PA 16509  
(814)868-9686

Interview, Workshop  
**Knoll, Sharon**  
**State Transportation Commission**  
955 Persimmon Court  
Fairview, PA 16415  
(814) 474-3644

Workshop  
**Kulich, Mark**  
**PA DCNR**  
Northwest Field Office  
1200 Lovell Place  
Erie, PA 16503  
(814) 871-4190

Workshop  
**Letkiewicz, Theresa**  
**Erie County Maintenance/Erie County**  
**Green TEEM**  
9031 Peach Street  
Waterford, PA 16441

Workshop  
**McGuire, Lorene**  
**Erie Metro Transit Association**  
127 E. 14th St.  
Erie, PA 16503  
(814) 452-2801

Interview, Workshop  
**Minnich, Tom**  
**PENNDOT, Environmental Manager**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7008

Interview, Workshop  
**Kissel, Edward**  
**S.O.N.S. of Lake Erie**  
801 West 21st Street  
Erie, PA 16502  
(814) 453-2270

Interview, Workshop  
**Kukla, Mark**  
**City of Erie**  
Municipal Bldg., Room 400  
Erie, PA 16501  
(814) 870-1373

Workshop  
**Lesser, Ed**  
**Erie Area Council of Governments**  
Erie County Courthouse, Rm 119  
140 West 6th Street  
Erie, PA 16501

Interview, Workshop  
**Maggio, Thomas**  
**Erie Western PA Port Authority**  
100 State Street  
Erie, PA 16507  
(814) 455-7557

Interview, Workshop  
**McMullen, Mike**  
**PENNDOT, District 1-0 Liaison Engineer**  
255 Elm Street  
Oil City, PA 16301  
(814)678-7077

Workshop  
**O'Brien, Tom**  
**GAI Consultants, Inc.**  
570 Beatty Road  
Monroeville, PA 15146-1300  
(412) 856-6400

Workshop  
**Petit, William**  
**PENNDOT, Maintenance Services Engineer**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7149

Workshop  
**Pirello, Anthony**  
**Erie School District**  
1330 W. 8th  
Erie, PA 16502

Workshop  
**Ralston, Bob**  
**SP&K Engineering, Inc.**  
126 Kaider Road  
Uniontown, PA 15401  
(724) 439-1600

Interview, Workshop  
**Rouch, Jake**  
**Erie Conference on Community Development**  
111 Boston Store Place  
Erie, PA 16501  
(814) 454-3878

Interview  
**Schreckengost, Deborah**  
**PENNDOT, Community Relations Coordinator**  
255 Elm Street  
Oil City, PA 16301  
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Interview, Workshop  
**Skellie, David**  
**Erie County Planning Department**  
Erie County Courthouse  
Erie, PA 16501  
(814) 451-6336

Workshop  
**Sokol, Anne**  
**Millcreek Township**  
3608 West 26th Street  
Erie, PA 16506  
(814)833-1111

Workshop  
**Pierce, Charles**  
**Zoning Officer-Millcreek Township**  
3608 West 26th Street  
Erie, PA 16506  
(814)833-1111

Workshop  
**Puskar, George**  
**PENNDOT**  
9031 Peach Street  
Waterford, PA 16441  
(814) 871-4411

Workshop  
**Ream, John**  
**PSU - Behrend College**  
Erie, PA 16563

Interview  
**Schreckengost, Ray**  
**Erie Western PA Port Authority**  
100 State Street  
Erie, PA 16507  
(814) 455-7557

Interview  
**Schwenk, Edna**  
**Neighborhood Watch**  
403 East Avenue  
Erie, PA 16507  
(814)455-7104

Workshop  
**Smith, Cindy**  
**PENNDOT, Roadside Development Spec.**  
255 Elm Street  
Oil City, PA 16301  
(814) 678-7147

Interview  
**Spaulding, Jeffrey**  
**City of Erie**  
626 State St., Municipal Bldg.  
Erie, PA 16501  
(814) 870-1270

Interview, Workshop

**Welsh, Jake**

**Erie County Planning Department**

Erie County Courthouse

Erie, PA 16501

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Interview, Workshop

**Yedinak, Brian**

**PENNDOT, Design Services Engineer**

255 Elm Street

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Interview, Workshop

**Wiley, Erin**

**PENNDOT, Transportation Planner**

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**Byers, Lisa**

**The RBA Group**

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## ACKNOWLEDGEMENTS

### **PENNDOT, DISTRICT 1-0**

PENNDOT, District 1-0 has worked to administer and facilitate this “Gateways and Greenways” effort for the concerned citizens of the Erie, PA region.

Special thanks to:

John L. Baker, P.E., PENNDOT, District Engineer

Karl Ishman, P.E., PENNDOT, Assistant District Engineer

### **ERIE COMMUNITY LEADERS**

This report recognizes the community leaders and concerned citizens of the Erie, PA region for their dedication in celebrating the unique character of Erie. Their enthusiasm and support has, and will continue, to make this project a success.

### **CONSULTANT TEAM**

Greenways, Inc. – Charles Flink, Haley Blakeman

The RBA Group – Annette Schultz, Lisa Byers, Pat Bachman

Trailblazers – Jeff Olson



## PHOTO CREDITS

*Cover (clockwise):* 1) US Brig Niagara. Lowery Photography. From website [www.brigniagara.org/brigniagara](http://www.brigniagara.org/brigniagara)

2) GoFish Sculpture. Erie, PA. Courtesy of PENNDOT, District 1. 3) View from State Street looking towards the Bicentennial Tower at Dobbins Landing. Courtesy of the Erie Western PA Port Authority. 4) Erie Nautical Flags-Logo.

*Page 2 Watermark:* U.S. Brig Niagara. Lowery Photography. From website [www.brigniagara.org/brigniagara](http://www.brigniagara.org/brigniagara)

*Figure 1.* Interstate 79, Erie, PA. Courtesy of The RBA Group.

*Figure 2.* Sketch of icons. Gateways and Greenways Workshop held May 10, 2001, Erie, PA.

*Figure 3.* Intersections. Erie, PA. Courtesy of The RBA Group.

*Figure 4.* Sketch of Streetscape. Las Vegas, NV. Courtesy of Trailblazer.

*Figure 5.* Public Art on a Sustrans trail. England. Courtesy of Trailblazer.

*Figure 6.* Conceptual Portrait of Erie. Gateways and Greenways Workshop held May 10, 2001, Erie, PA.

*Figure 7.* Logos. From website [www.porterie.org](http://www.porterie.org) and report titled *A Comprehensive Visitor Sign System and Interactive Kiosks for the Greater Erie Region*, Erie Signage Committee, Informing Design, Inc.

*Figure 8.* Bayfront aerial view. Courtesy of the Erie Western PA Port Authority. Vineyards near Erie and Dobbins Landing view. Courtesy of The RBA Group.

*Figure 9.* Wall treatment, Squaw Peak Parkway, Phoenix, AZ. From the website [www.harriesheder.com/oco.htm](http://www.harriesheder.com/oco.htm)

*Figure 10.* Gateway Elements. Indianapolis, IN. From website [www.brickyard.com/press/hulman-121699.html](http://www.brickyard.com/press/hulman-121699.html)

*Figure 11.* Wall Treatments. Pima Freeway, Scottsdale, AZ. From website [www.scottssystem.com/brosum.htm](http://www.scottssystem.com/brosum.htm)

*Figure 12.* Sentinel I, Sculpture on the Sustrans Cycle Path and Sculpture Trail: Bristol and Bath, England. From website [www.shu.ac.uk/services/lc/slidecol/pubart/other/sustrans\\_b~b/index.html](http://www.shu.ac.uk/services/lc/slidecol/pubart/other/sustrans_b~b/index.html)

*This Page.* Photo of Workshop Participants. Gateways and Greenways Workshop held May 10, 2001, Erie, PA.